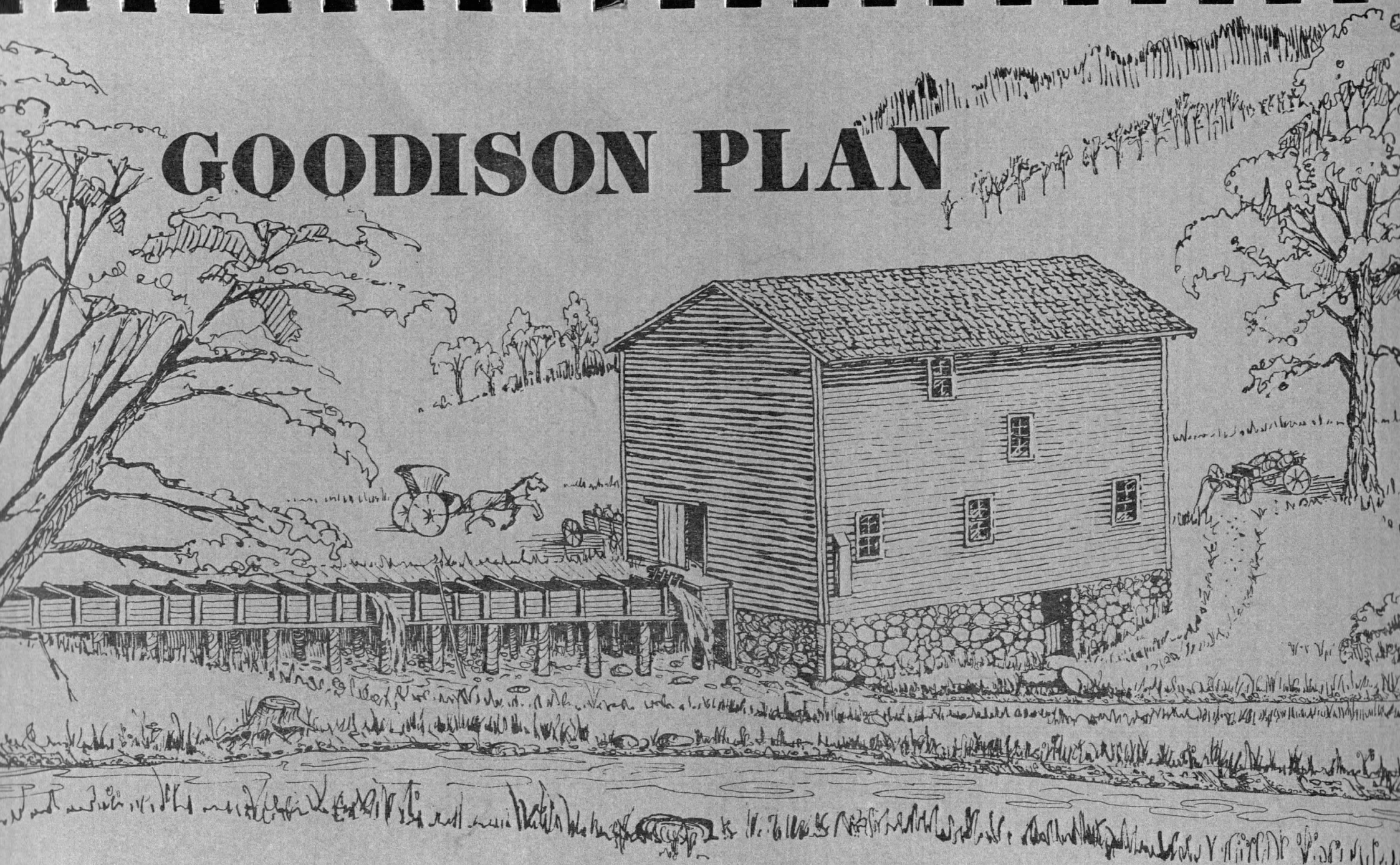


GOODISON PLAN



HEMMINGWAY MILL • Circa 1848

Township of Oakland
OAKLAND COUNTY, MICHIGAN

ACKNOWLEDGMENT

This planning project was initiated and officially authorized during the year 1977 by the Oakland Township Board of Trustees.

Alfred J. Taylor, Supervisor

Carolyn L. Phelps, Clerk

Marilyn M. Malinowski, Treasurer

Kent B. Kelly, Trustee

Alice W. Tomboulion, Trustee

Cover artwork and research by Frank Dulin, Oakland Township



BILLS/CHILDS ASSOCIATES, P.C.

5221 COMMERCE ROAD, ORCHARD LAKE, MICHIGAN 48033 313 - 682-7644

SITE DEVELOPMENT PLANNING • URBAN DESIGN • LANDSCAPE ARCHITECTURE
GOLF COURSE ARCHITECTURE • PARK PLANNING • ENVIRONMENTAL DESIGN

PAGE NUMBER

Alfred J. Taylor, Supervisor
Township of Oakland
4393 Collins Road
Rochester, Michigan 48063

July 8, 1980

Re: Goodison Plan, Phase 2

Dear Mr. Taylor:

It is my pleasure to submit to you and the Township Board this completed Phase of the Goodison Plan. This text and plan represents the culmination of our combined efforts toward achieving the most appropriate progression of planned improvements and/or growth for the Goodison area.

Because of Goodison's strong historical heritage is so desirable for producing a unique neighborhood center, the challenge is to discover a method for adequately planning the preservation of this heritage while meeting the desired needs of Goodison area citizens. It is hoped the implementation of this plan will meet that challenge.

Sincerely yours,
BILLS/CHILDS ASSOCIATES, P.C.

ROBERT W. BILLS, R.L.A.
President

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BILLS/CHILDS ASSOCIATES, P.C.

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2
4
4
7
8
9
10
11
12
13
15
17
17
18
19

INDEX

	<u>PAGE NUMBER</u>
ACKNOWLEDGMENT	
LETTER OF INTRODUCTION	
HISTORY OF THE GOODISON SETTLEMENT	1
THE "GOODISON IDEA"	2
ISSUES AND DESIGN CONCEPTS PHASE TWO	4
1. Traffic	4
2. Parking	7
3. Walkways/Bikeways	8
4. Industry Land Use Relationships	9
5. Commercial Expansion	10
6. Schools	11
7. Residential Use	12
8. Historical Preservation	13
9. Environmental Resources	15
PLAN PRIORITY RECOMMENDATIONS AND WORK PROGRAM	17
1. Short Term Plan (1 to 5 Years)	17
2. Long Range Plan (5 to 25 Years)	18
CONCLUSION	19

Regional Location

15 MILES

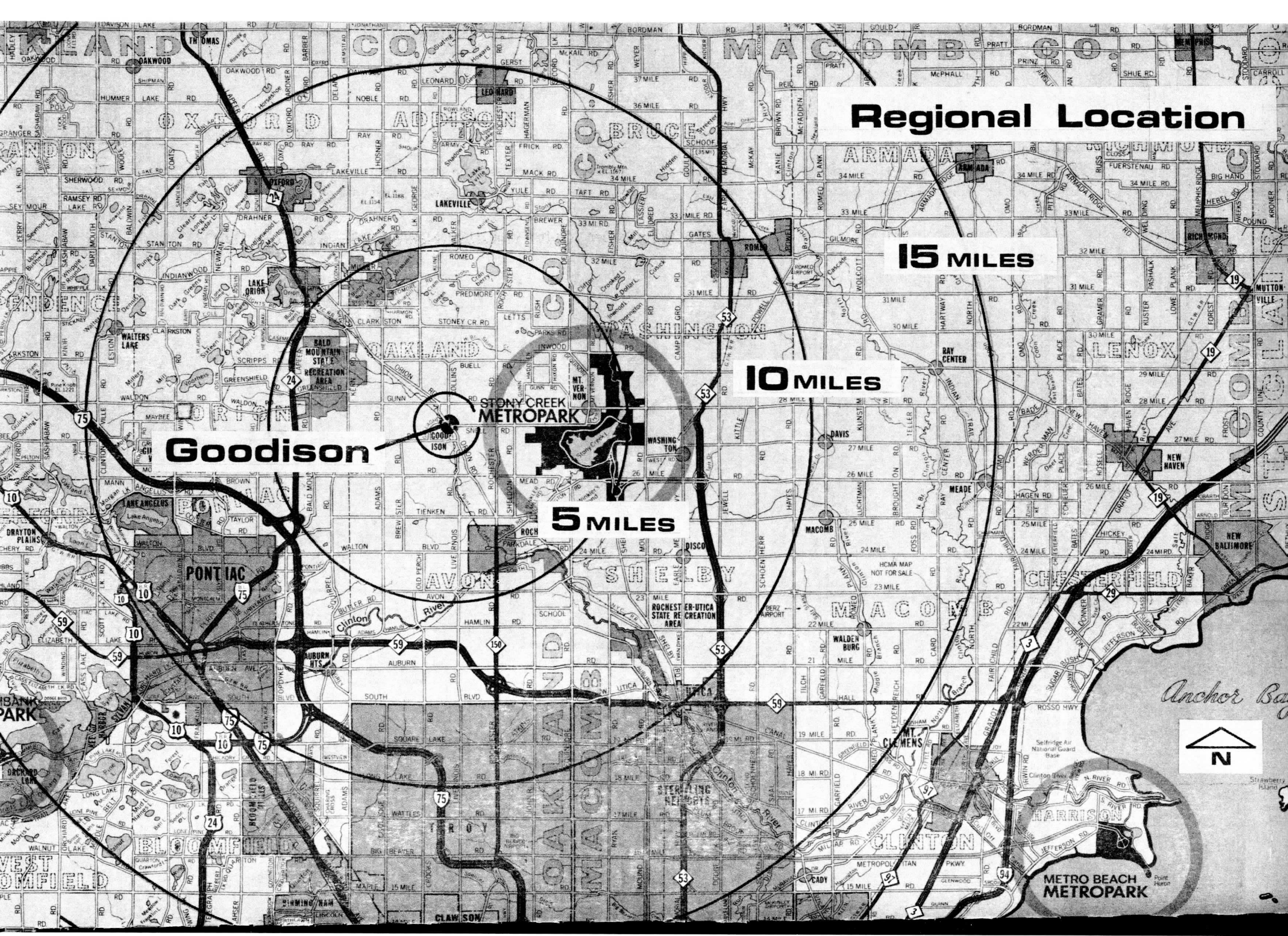
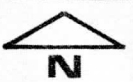
10 MILES

5 MILES

Goodison

STONY CREEK METROPARK

METRO BEACH METROPARK



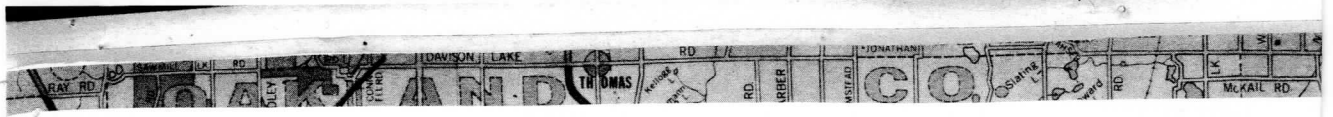
HISTORY OF THE GOODISON SETTLEMENT

Similar to a number of early settlements throughout the Oakland County area, the Paint Creek Valley is well endowed with remnants of the region's past. Hardy pioneers migrating from eastern states during the early 18th Century found the Valley within Oakland Township's pleasant environment an ideal location in which to establish their homes and enterprises. Fertile soils, streams, lakes, and forest stands provided attractive resources for the settlers' basic needs.

Needham Hemingway and his two brothers-in-law, Asa Baker and Benedict Baldwin arrived in the wilderness area that is presently Goodison during the year 1825. Needham purchased 80 acres from the Michigan Territorial Government and proceeded to construct a mill race to power a grist mill he built in 1835. The present Paint Creek Cider Mill and parking lot cover the former Needham Mill location and the original race remains extending north from Gallagher Road along Orion Road. The mill operated under various owners for 100 years grinding grain until as late as 1941 and continuing to utilize its water powered machinery for Crate and Wheel Rim Manufacturing. The entire structure was razed in 1948, it's lifetime spanning 113 years.

One of the mill's owners was William Goodison, who in 1872 purchased the building from Needham along with his house and land holdings. At this time the Detroit-Bay City Railroad was constructed through the Paint Creek Valley providing area farmers with transportation for their produce shipments. The railroad station near the mill also served as the community post office, with William Goodison, Jr. as its postmaster. Area residents at the time referred to this train stop as "Goodison's Station" and the present day Goodison derived its label from these origins.

As Goodison developed to a center of an agrarian community, the mill and railroad were joined by a general store, blacksmith shop, sawmill, one room school, and the Paint Creek Methodist Church; all serving the needs of the community. Some of these structures such as the Needham Mill Race, the Methodist Church, Paint Creek Market and several residences remain today.

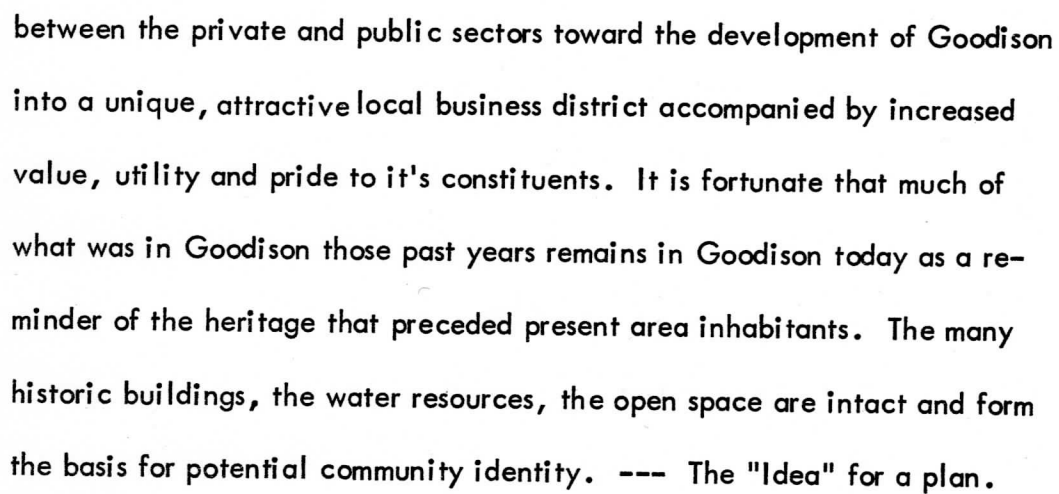


THE "GOODISON IDEA"

From it's creation in the 1830's, Goodison has functioned as a small but important center of commerce for the surrounding rural community. It remains a commercial hub for the Paint Creek corridor and will in all probability continue in this direction as the reduction in farming operations gives way to moderate and low density residential use.

Goodison's historical importance together with it's logical future growth has been recognized for some time by the Township administration, and the present Paint Creek Valley growth of low density communities necessitates that Goodison maintain it's "neighborhood scale" characteristics insofar as possible.

Goodison's growth through the 1930's, 40's, and 50's has, like many other small communities throughout the region been largely uncontrolled. The Township completed a Comprehensive Development Regulation Plan in 1974 that established a basis for orderly planning. During 1977 the Oakland Township Board expressed concern over Goodison's future and just prior to the closing months of that year initiated a course of action. During November of 1977 a meeting was held with Goodison businessmen in order to establish a dialogue



During May of 1978, Oakland Township received notification from Oakland County that Federal Funding was available through community block grants for planning within the Township. The Goodison area was given a high priority by the Township Board and during the summer of 1978, work on gathering required base data was completed toward the formation of the Goodison Plan, Phase 1. The Goodison area has no recorded village limits delineating it as a separate legal entity. The area considered by local residents to include Goodison as such covers approximately 80 acres bounded on the north by Baldwin School, the south by the old railroad right-of-way, the east by the

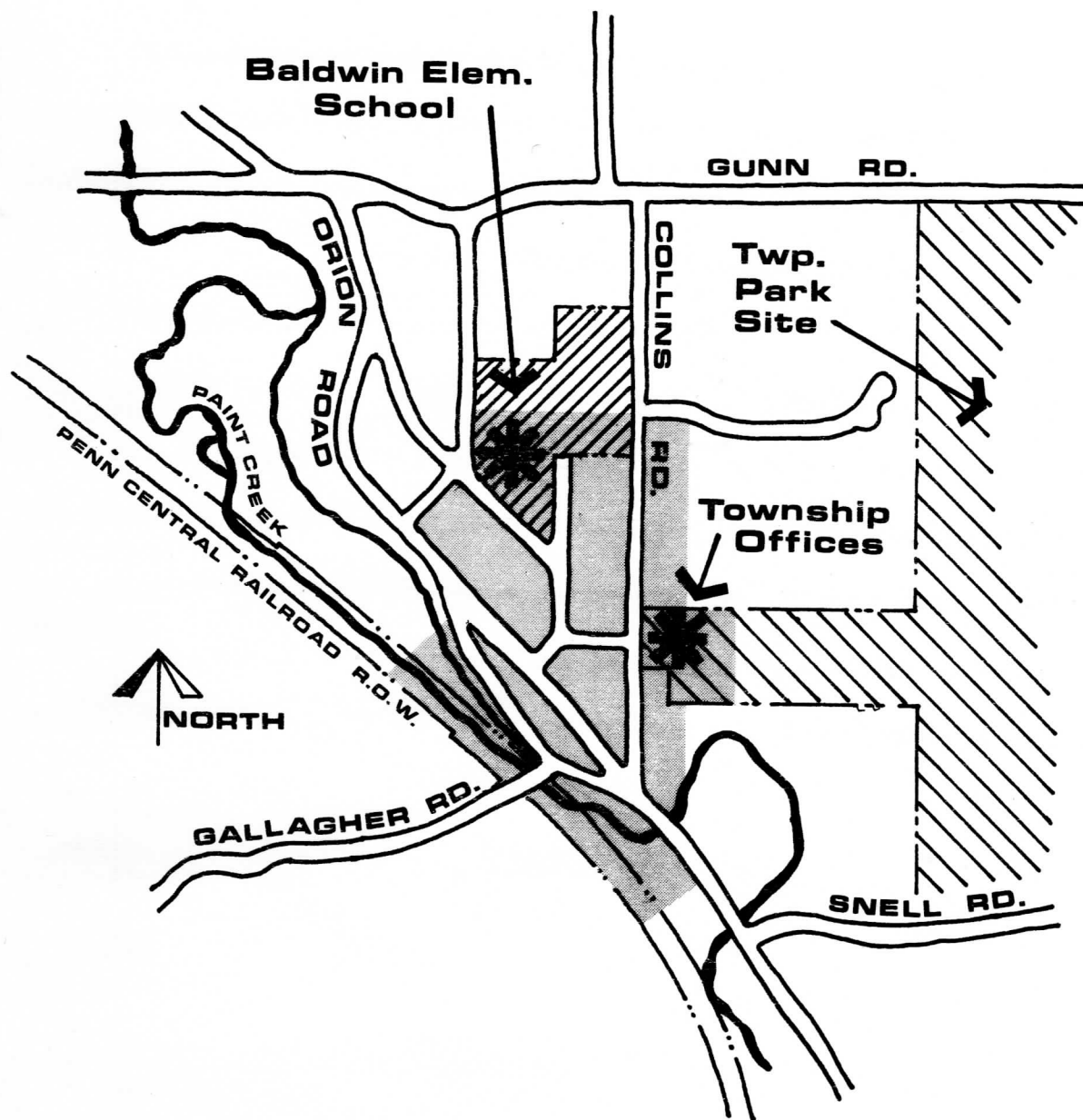
THE "GOODISON IDEA" (continued)

residential properties fronting on Collins Road (including Town Hall) and the west by Orion Road (see map on the right). For the purpose of the Goodison Plan, these locations have been set as the project study limits with consideration given to certain impacts from surrounding land use.

Phase 1 which forms the basis for the total Goodison planning project identified area problem locations and zoning conflicts. By the end of 1978, this Phase was completed and presented to the Township Planning Commission in preparation for subsequent public hearings. During April of 1979 the Phase 1 general design concept and recommended zoning changes were publicly reviewed and citizen support encouraged the Township Board to authorize further planning under Phase 2.

Study Area





ISSUES AND DESIGN CONCEPTS PHASE TWO

Research conducted during the Phase 1 period together with added reconnaissance during the summer of 1979 identified ten separate categories that were addressed as problem areas within Goodison's day to day existence. These are as follows:

1. Traffic and Transportation
2. Parking
3. Walkways/Bikeways
4. Industry Land Use Relationships
5. Commercial Expansion
6. School Site Relationship
7. Residential Area Preservation
8. Historical Structure Preservation
9. Natural Environmental Resources

Throughout the balance of this text each of the above categories are discussed principally in terms of their present status followed by design recommendations for improvements. In some instances alternative recommendations are also offered that may be deemed more practical or more adaptable to phased implementation.

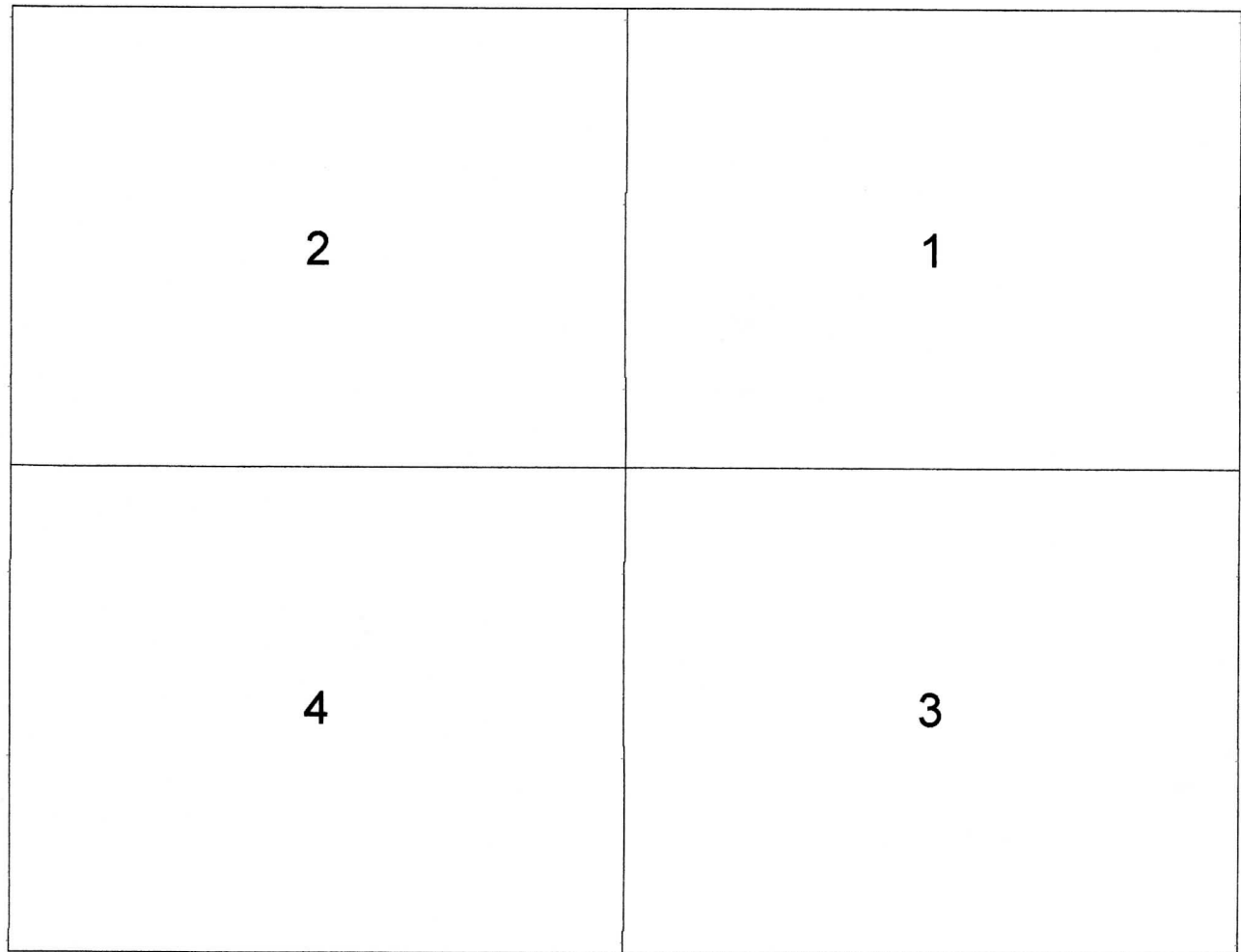
TRAFFIC

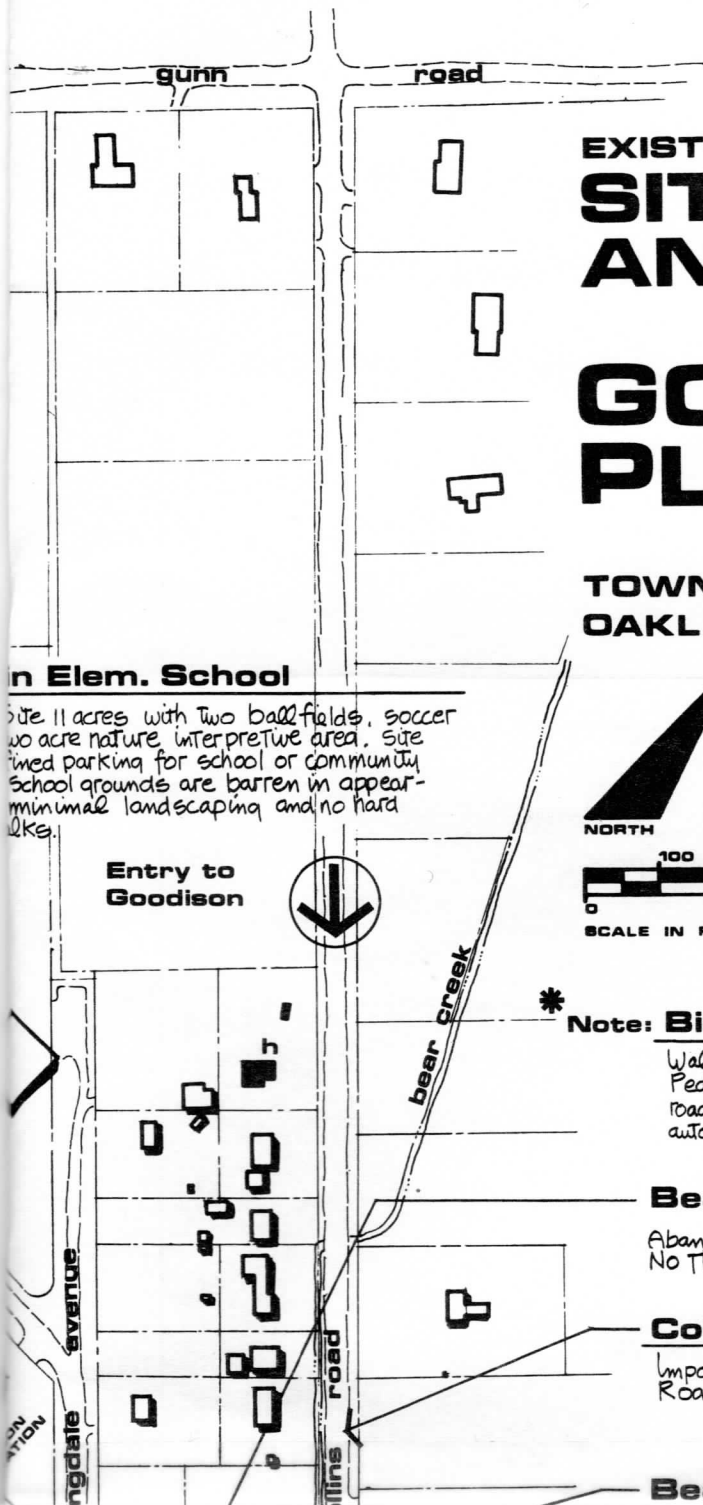
Existing Conditions

Unlike Goodison's earlier days, residents presently depend totally on the private automobile for transportation both within and around Goodison. It is interesting to note that from 1900 to 1930 Goodison residents had three transportation alternatives; private horse drawn vehicles and later automobiles were supplemented during this period by the railroad and an electric trolley line that connected Goodison with Detroit, Flint, Saginaw, and Bay City. The trolley line operated by the Detroit Union Railway Company became a very useful commuting system often utilized by youngsters traveling to high-schools in Rochester and Lake Orion.

To date, Orion Road has a major impact on Goodison's daily functions and as a major regional traffic carrier is crucial to Goodison's development. This road corridor remains with a number of scenic characteristics and provides a pleasant drive through the southern sector of Oakland Township.

The next four pages are four quadrants of a large blueprint size drawing. Print and assemble as below:





EXISTING CONDITIONS

SITE ANALYSIS

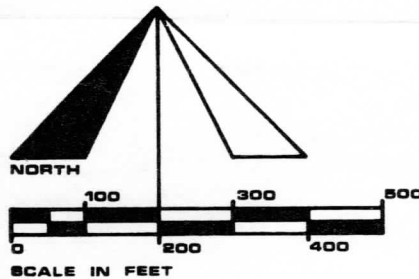
GOODISON PLAN

TOWNSHIP OF OAKLAND
OAKLAND COUNTY, MICHIGAN

n Elem. School

Site 11 acres with two ball fields, soccer
two acre nature interpretive area. Site
lined parking for school or community
School grounds are barren in appear-
minimal landscaping and no hard
walks.

Entry to
Goodison



* **Note: Bicycle/Pedestrian Paths**

Walks or walkways are totally non-existent
Pedestrians and bicyclists are forced to utilize
roadway rights of way designed solely to accomodate
automobile traffic.

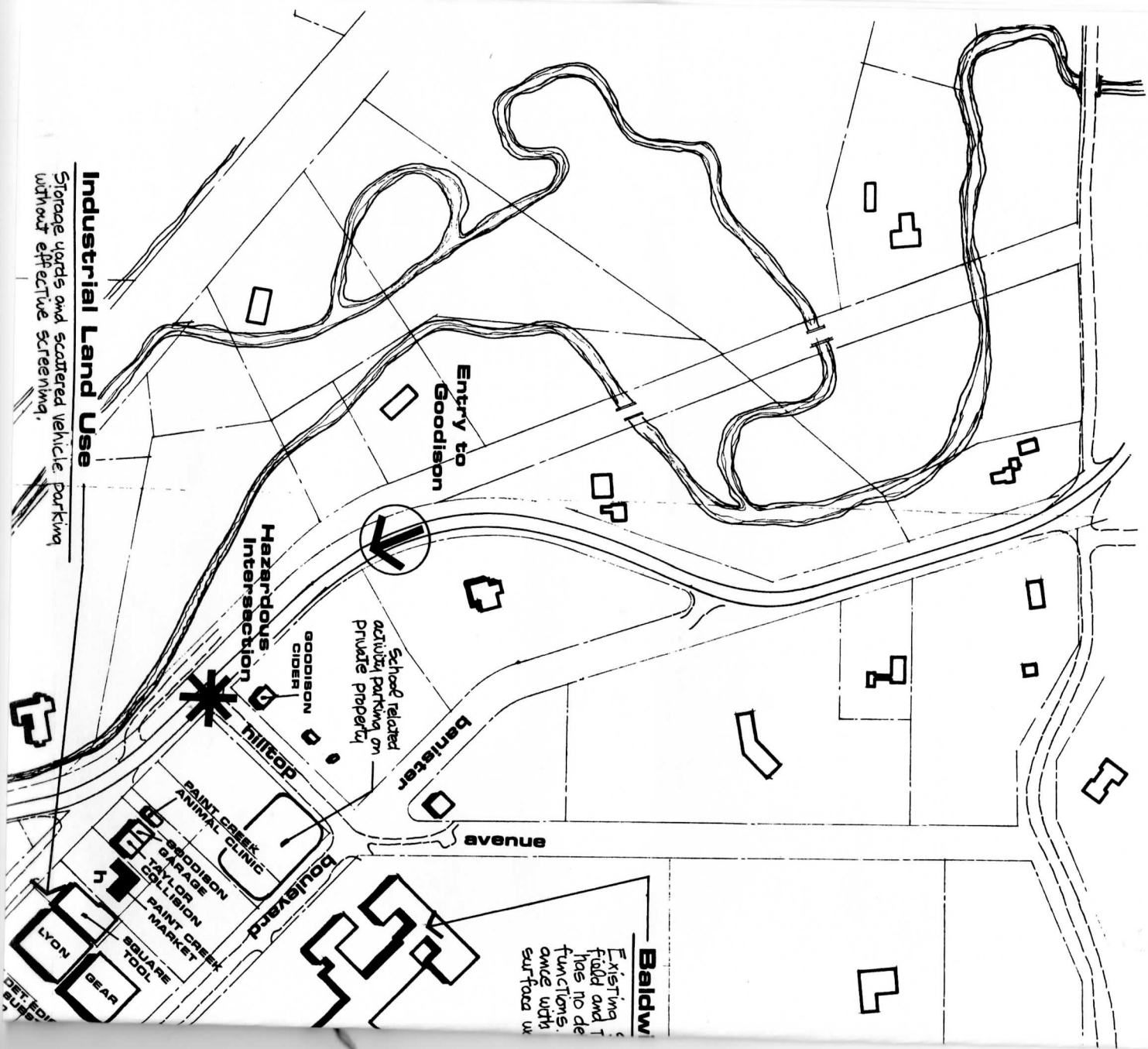
Beatrice Avenue

Abandoned weed and brush covered right of way
No Through access.

Collins Road

Important North/South connector to Orion
Road. Does not relate well to other streets.

Bear Creek



Baldwin
 Existing field and has no definite functions. surface in

Industrial Land Use
 Storage yards and scattered vehicle parking without effective screening.

PAINT CREEK ANIMAL CLINIC
 GOODISON GARAGE
 TAYLOR COLLISION
 PAINT CREEK MARKET
 SQUARE TOOL
 LYON
 GEAR
 DET. ED. SUPER

prohibits pedestrian access to Creek and Mill race. Vegetation and Orion Road acts as a buffer traffic and adjacent natural resources.

Orion Road

A major regional traffic carrier with scenic character through southern sector of the Township. Problems result from winding alignment and high motorists approach speeds at intersections in town.



Note: Automobile Parking

Parking needs for existing commercial and manufacturing operations are totally lacking in organization, and can not be readily identified by motorists unfamiliar to the area.

Regional Bikeway

Abandoned Penn. Central Railroad Right of way. Has potential as a regional pedestrian/bike path corridor.

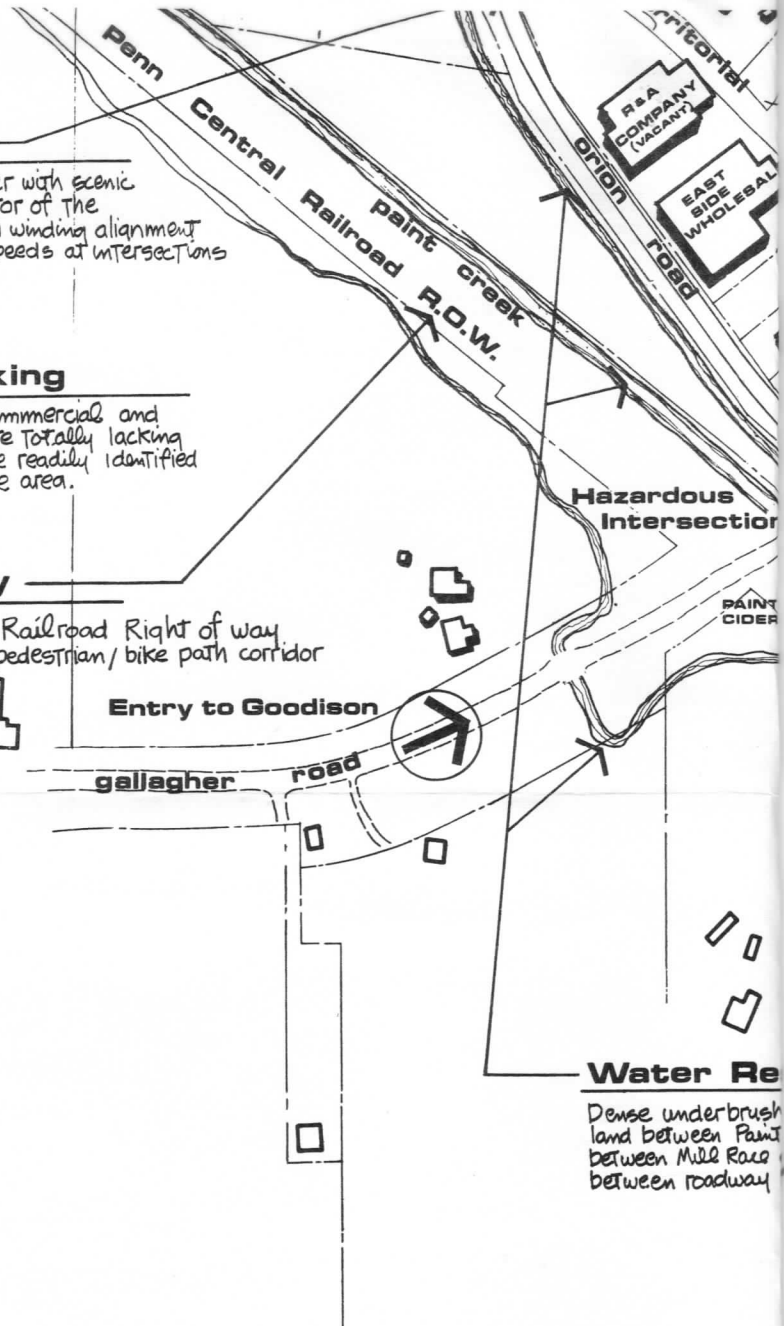
Entry to Goodison

gallagher road

Hazardous Intersection

Water Re

Dense underbrush land between Paint Creek and Mill Race between roadway



ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

TRAFFIC

Existing Conditions (continued)

The segment of Orion Road passing through the Goodison area has presented a number of traffic problems primarily from motorists failing to slow their speed when entering the area. The problem is compounded by Orion Road's winding alignment and the presence of hazardous intersections at Hilltop, Gallagher and Collins Roads. Orion Road can be expected to continue as a major thoroughfare through Goodison with local streets such as Banister and Territorial handling local circulation. Collins Road, an important north/south connector to Orion does not relate well to Goodison's other streets having no local connection other than a constricted alley type access over the old Territorial Road right-of-way along the rear of the Rochester Gear Plant. The Territorial Road intersection with Collins at this point also offers a hazardous situation due to it's proximity to the busy Collins/Orion Road intersection.

Other road problems worthy of consideration include lack of definition at the Territorial/Orion Road intersection in front of the Paint Creek Market and

Goodison Garage. School bus access to Baldwin School from the Collins Road corridor is also a concern since bus traffic must either attempt to use heavily traveled Orion Road or squeeze through on Territorial behind the Rochester Gear Plant, often finding it blocked by parked plant employee autos.

Design Recommendations

With regard to Orion Road, it is imperative that some method or device be utilized to slow regional traffic to acceptable speeds when traveling through Goodison. Occasional speed traps set up by county law enforcement agencies are only a partial answer and cannot be consistent due to the county wide demands on enforcement departments. Alterations in Orion Road's alignment have been previously studied including rerouting west bound traffic on to Territorial leaving only the east bound flow on the present road. This approach proved to be unpopular with the Oakland County Road Commission primarily due to cost and because it would result in the movement of adverse regional traffic effects even further into Goodison's center.



ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

TRAFFIC

Design Recommendations (continued)

A more rational approach would be to develop a solution through effective warning signs and flashers along Orion Road at the northwest and southeast entry areas to Goodison. These should be positioned at a sufficient distance out to allow approaching motorists ample time to slow prior to reaching the Collins or Hilltop intersections. The addition of control devices at either of these intersections would also be a significant factor toward reducing Orion Road speed hazards and improving traffic circulation on local roads.

The lack of convenient access from Collins Road to the west sector of Goodison can be alleviated by opening the unimproved Beatrice Avenue directly off of Collins Road opposite the Township Hall parking area. This road connecting Springdale Avenue was in use many years ago but was never improved leaving only a brush covered corridor within a recorded 60 foot right-of-way. Proposals for improving this road with gravel surfacing and an adjacent walkway are presently under consideration by the Township Board.

The opening of Beatrice Avenue should be followed by the closing of Territorial Road at Collins, eliminating a hazardous situation near the intensively used Collins/Orion intersection. Territorial would then terminate at a proposed parking lot at the back of the Rochester Gear Plant.

The Territorial/Orion intersection in front of the Paint Creek Market requires a more obvious definition and this can be accomplished by enlarging the existing curbed island to effective proportions. Studies on this approach have been completed to date and reviewed in cooperation with the owners of abutting businesses.

The Hilltop Road intersection at Orion Road is particularly dangerous due to poor sight distances from the north as cars traveling south on Orion approach the intersection on a curve. The vacant property on either side of Hilltop at this point is zoned commercial making it feasible to relocate the road further south as a part of future business development. An alternative approach would be to vacate Hilltop entirely, moving local access to Orion Road on Territorial and Collins Road.

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

PARKING

Existing Conditions

At the present time automobile parking needs for existing commercial and manufacturing operations within Goodison are totally disorganized. The only locations that become obvious as places to park occur in front of the Paint Creek Market, the Township Hall lot and the Paint Creek Cider Mill area. Parking for Baldwin School functions occurs on vacant private property south of Bannister Boulevard in front of the school. Manufacturing plant employee cars such as Rochester Gear and Lyon Gear park along either Territorial or Bannister frequently restricting traffic. Baldwin School bus parking is also somewhat scattered northwest of the school.

Design Recommendations

The "Goodison Plan - Phase 1" map shows specifically designated parking areas in relation to proposed land uses. The parking designation immediately south of Beatrice Avenue is intended to serve a proposed commercial zone on Collins together with employee parking for manufacturing on the south. Access

would be from Territorial, Springdale and Beatrice. Another sizable parking site is recommended east of Collins to also serve proposed commercial establishments on Collins Road. No parking would be permitted on Collins Road. An area east of Baldwin School is shown as parking for school purposes together with the proposed "Village Green" Community Park to the south. Access is at the north extension of Springdale Avenue.

Recommendations for the physical design of proposed parking area locations should be somewhat standardized throughout Goodison, particularly with regard to lighting and landscape treatment. The construction of a flat monotonous gravel surface is not recommended as an end product. Designs at all locations should include hard surfacing, turfed curb islands, berm screens, shade tree plantings, decorative lighting, and convenient access. The recently constructed Township Hall parking facility on Collins Road is a good example. Wherever possible parking areas required to serve business establishments should be located to the side or rear of the buildings and not fronting on the local or regional thoroughfare.

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

WALKWAYS/BIKEWAYS

Existing Conditions

Anyone attempting to walk from their residence in Goodison to any point in the commercial or school area soon realizes they should drive if only for safety purposes. Walks or walkways are totally non-existent in Goodison causing residents who may occasionally choose or are obligated to walk to a local destination to share roadways with vehicles. The same situation applies to bicycle riders. A system of local walkways within the Goodison area could provide residents with a pleasant and safe alternative for pedestrian circulation or bike riding for practical as well as recreational purposes.

Design Recommendations

Walkways and/or bike paths do not necessarily need to parallel vehicular roadways although this practice is common largely due to the right-of-way availability. Goodison has the potential for establishing an excellent walk/bikeway system that would not only follow certain existing streets but at a number of points parallel Paint Creek, the Old Mill Race, and circulate through a proposed

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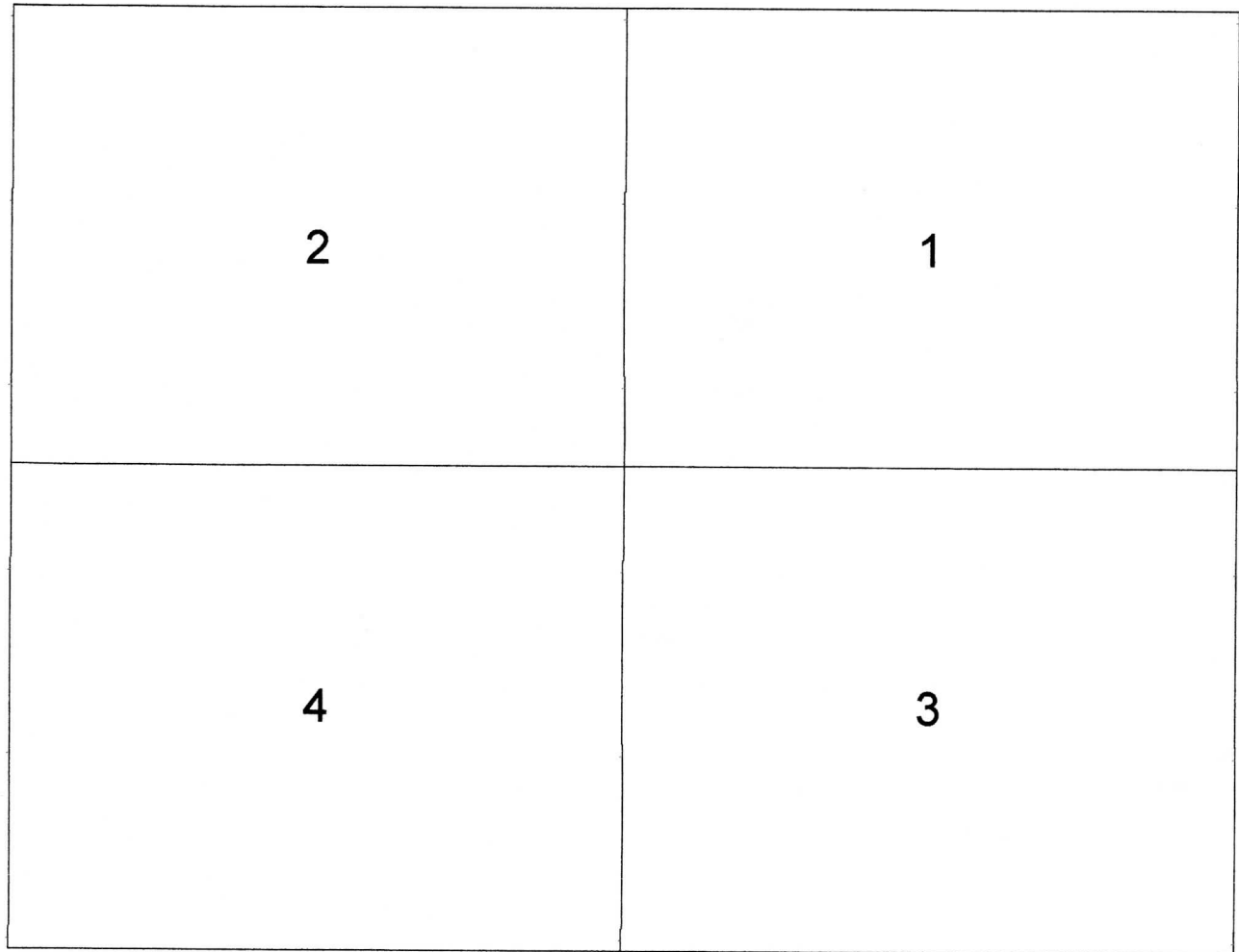
"Village Green" area at Springdale Avenue east of Baldwin School. To date a walkway plan has been completed for Goodison covering approximately two miles of walkways including three timber footbridges on Paint Creek and three terraced sitting areas. Based on the completed Phase 1 Concepts of the Goodison Plan, applications have been submitted by the Township for community development block grant funds to design and construct pedestrian/bikeways. As of December 1979, \$43,300.00 for this purpose has been approved. A high priority has been placed on an initial walkway segment to be constructed concurrently with the Beatrice Avenue improvement project, presently scheduled for construction during the Fall of 1980. Grant money will also be applied to footbridge construction at proposed Paint Creek crossings, and to walk plaza intersections.

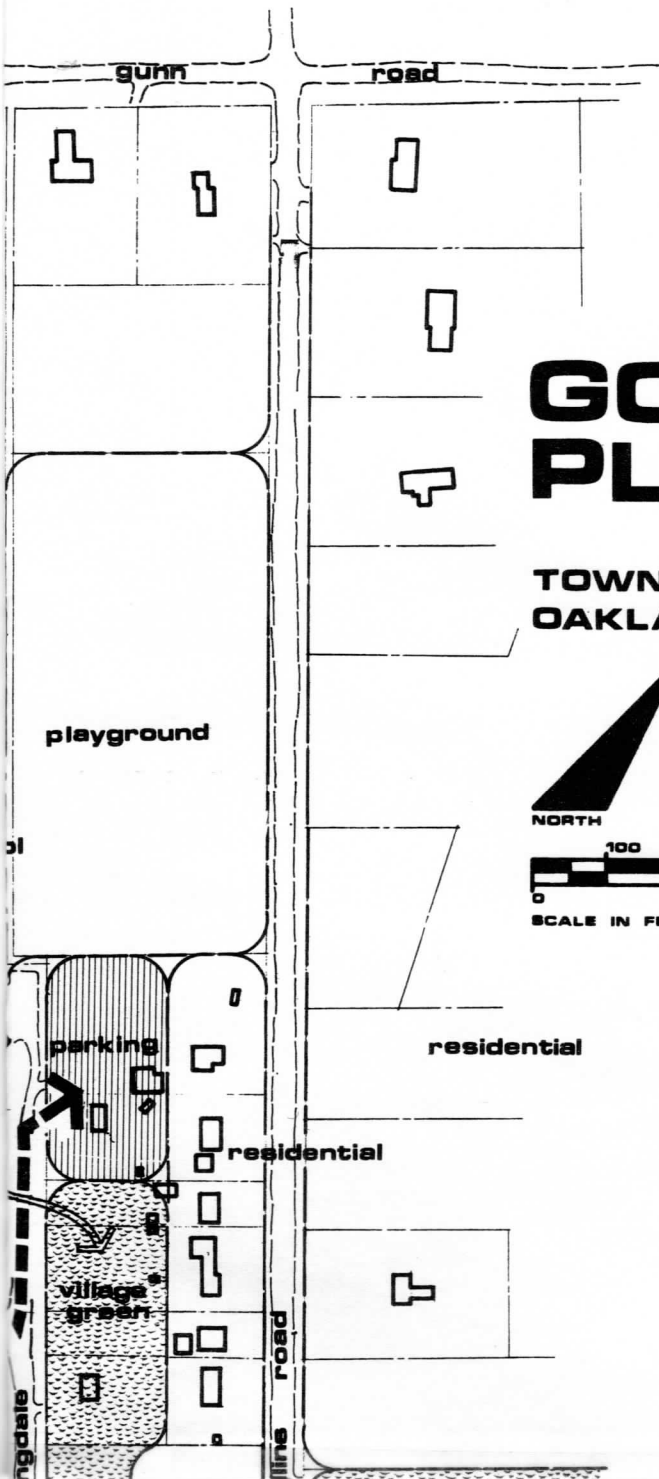
Recommendations for basic walk design should consider the following:

1. Hold walk widths to a 6 foot minimum to comfortably accommodate pedestrian groups, bicycles, and adapt to snow removal equipment.

SA

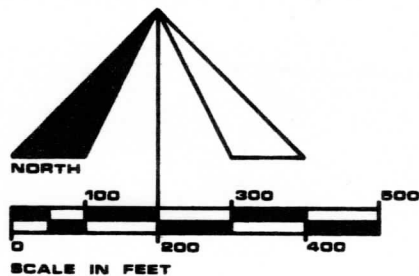
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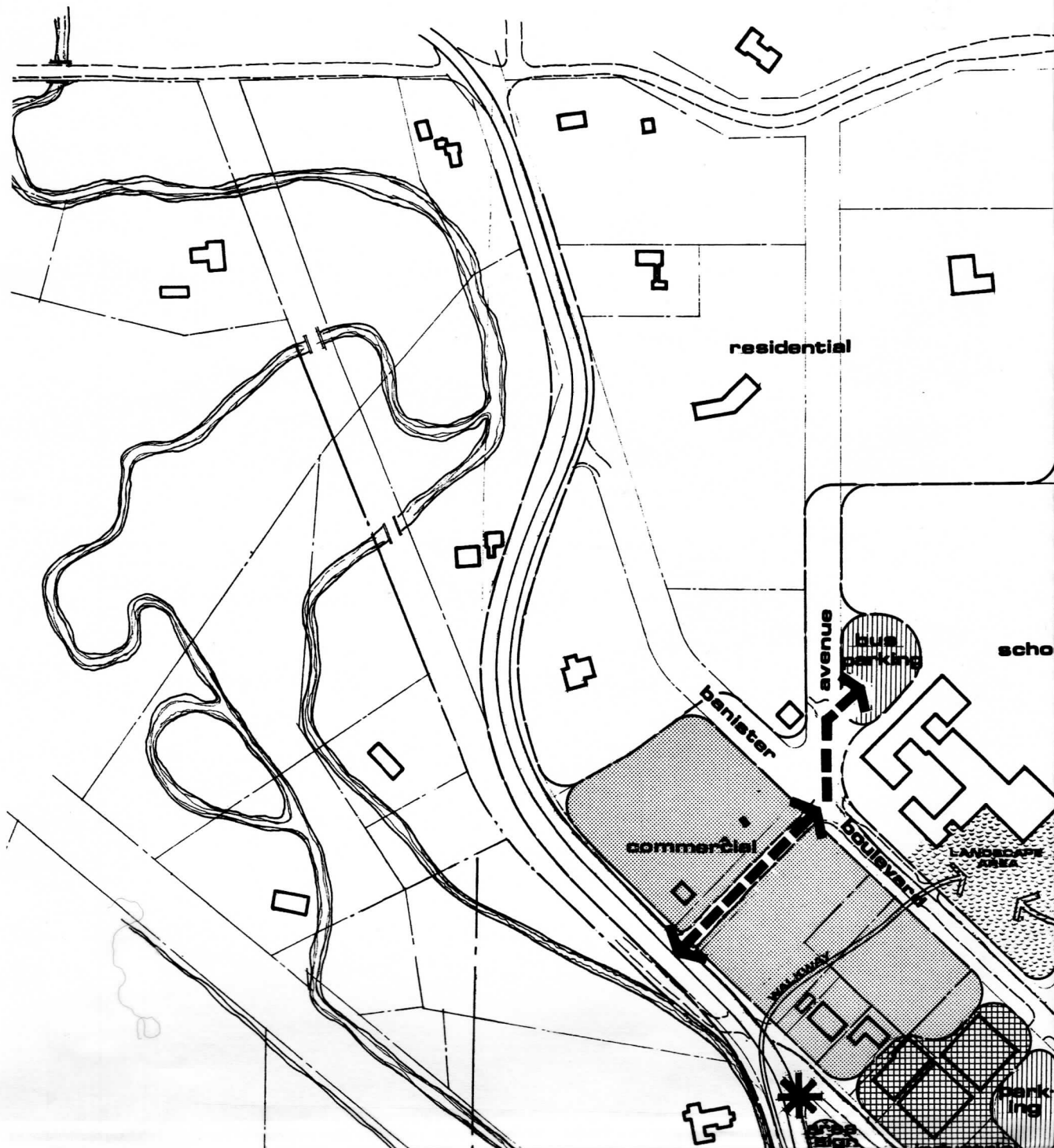


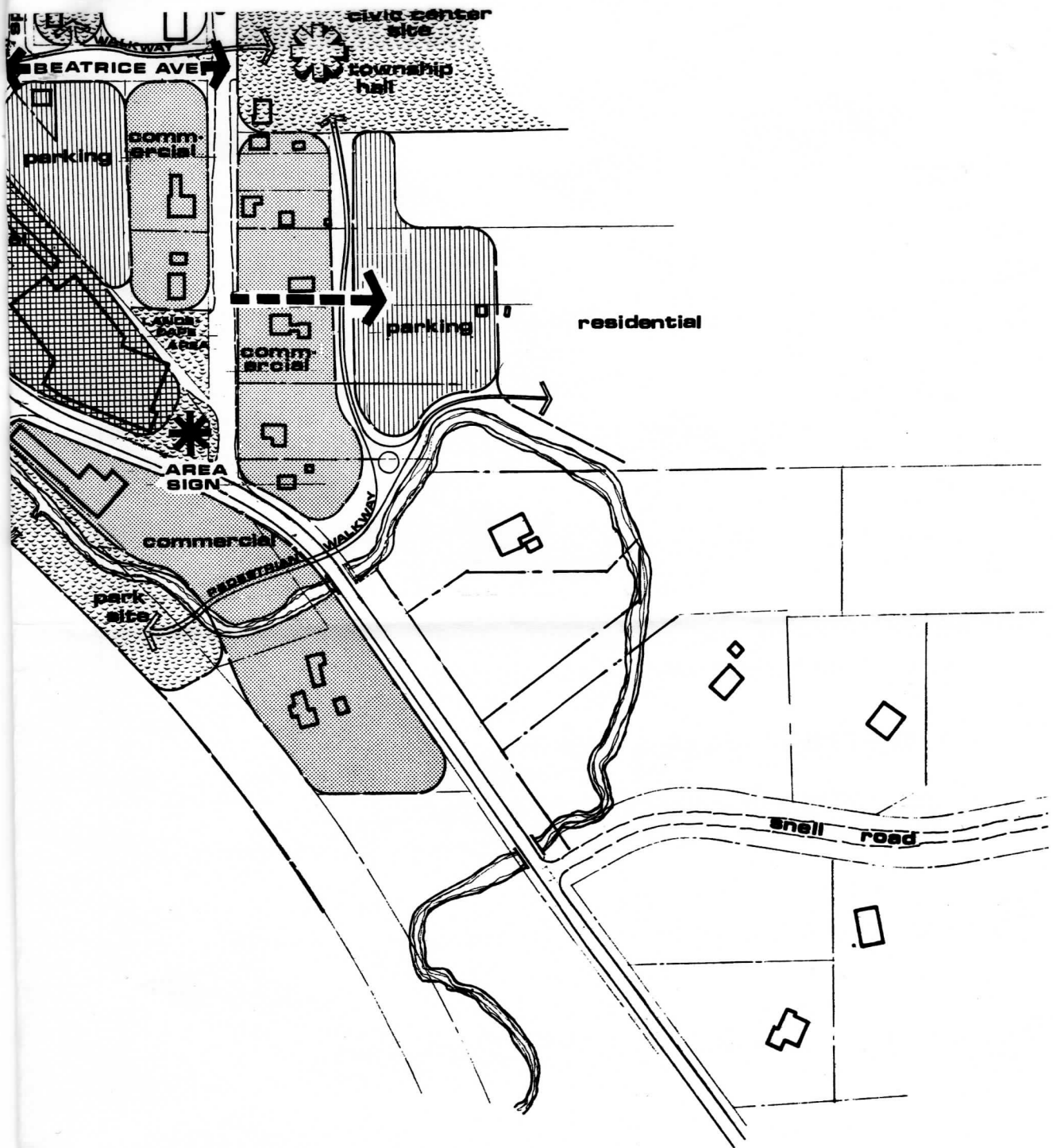
GOODISON PLAN-PHASE 1

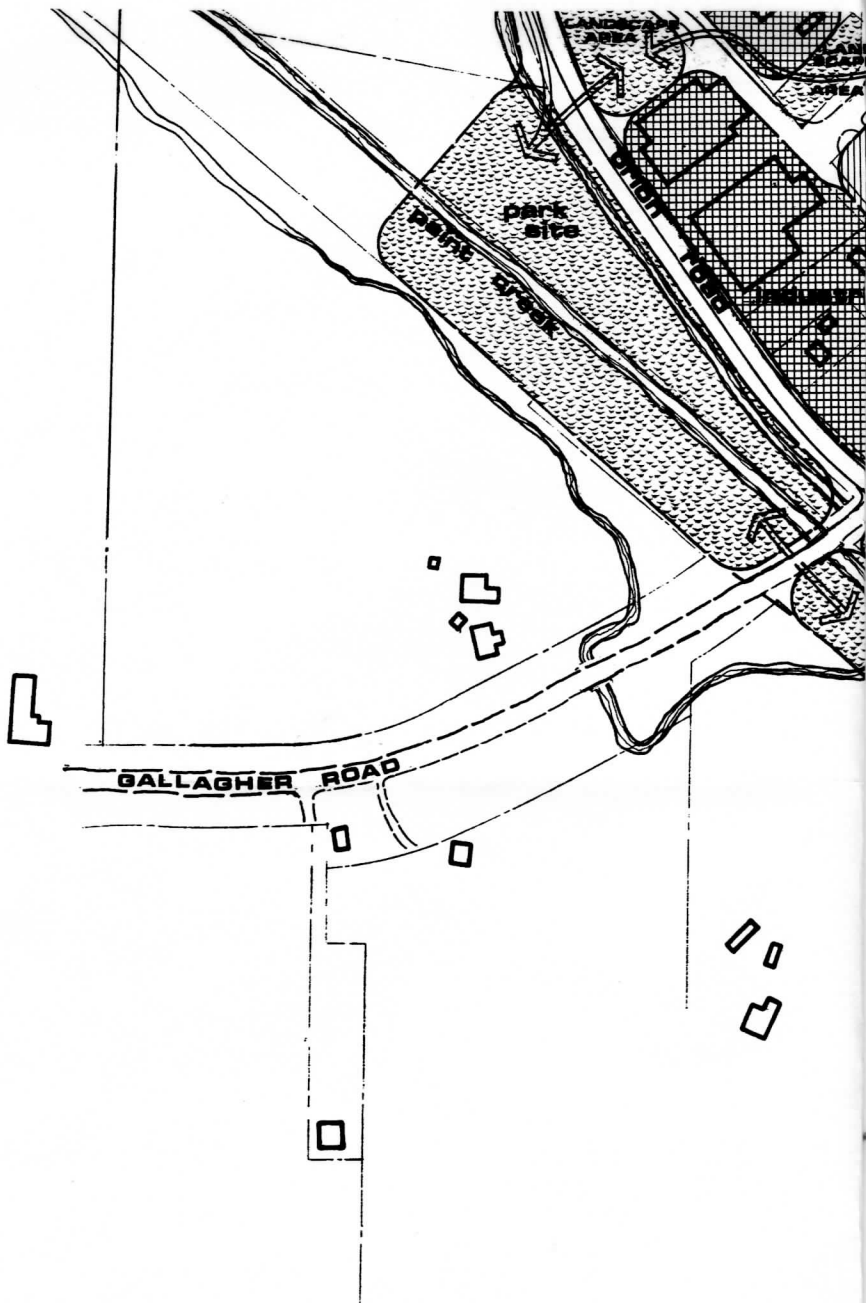
TOWNSHIP OF OAKLAND
OAKLAND COUNTY, MICHIGAN



MARCH 1, 1979







ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

WALKWAYS/BIKEWAYS

Design Recommendations (continued)

2. Vary walkway alignments wherever possible. "It's a long road that has no turning" also applies to walk design. Meandering walks are more desirable to users than a monotonous straight-a-way.
3. Utilize a variety of surfacing. Although asphalt surfacing is the predominate material in use today for walks in urban areas, other materials such as pressure treated wood and concrete are, under certain applications, worthy of consideration. Since the introduction of chemically treated wood materials to prevent rotting, boardwalk construction is now possible and can be a desirable variation to paving materials. Boardwalks at the turn of the century were popular in many towns where wood was plentiful and these historical dimensions would adapt well to Goodison. Boardwalk type construction is recommended for Collins Road and in front of the Paint Creek Mill due to the significant historical character of these sites.

Asphalt surfacing is recommended for the larger percentage of proposed Goodison walkways with concrete surfacing recommended for plaza intersections.

INDUSTRY LAND USE RELATIONSHIPS

Existing Conditions

Almost 90% of Oakland Township's industrial land use is located in

9

Goodison; initiated in 1837 when Needham Hemingway constructed his grist mill on Paint Creek. At the present time, 4 acres of the Goodison project area or 5% is occupied by light manufacturing establishments. These principally include Rochester Gear, Square Tool Company, Lyon Gear, and East Side Wholesale Company. A Detroit Edison transformer yard covering 2.5 acres is a part of the industrial area situated at the intersection of Springdale and Territorial Roads.

The physical appearances of present manufacturing operations from both the front and back approaches is a major factor toward influencing the ultimate character of immediately adjacent land uses. Unsightly storage yards, scattered vehicle parking and bulk delivery areas facing Territorial and Springdale Avenues are presently without effective screening and distract from Goodison's existing neighborhood character.

Design Recommendations

A critical assumption of the Goodison Plan is that the above industries

9A

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

INDUSTRY LAND USE RELATIONSHIPS

Design Recommendations (continued)

will continue operations and some will wish to expand as in the recent case of Lyon Gear. Under present zoning classifications there is little or no space for significant expansion and the neighborhood character of Goodison would certainly be jeopardized if such expansion is encouraged. It can also be assumed that the most serious impacts would be on those surrounding uses which are least similar, primarily residences and Baldwin School. Solutions toward mitigating such impacts can be achieved by the addition of factory storage yard and/or service area screening, properly designed and landscaped parking lots, and acceptable levels of maintenance to buildings and grounds. The "Industrial Park" concept so successful in other communities would, if applied to Goodison's industries, provide considerable support to plans for reinforcing the project's neighborhood scale.

COMMERCIAL EXPANSION

Existing Conditions

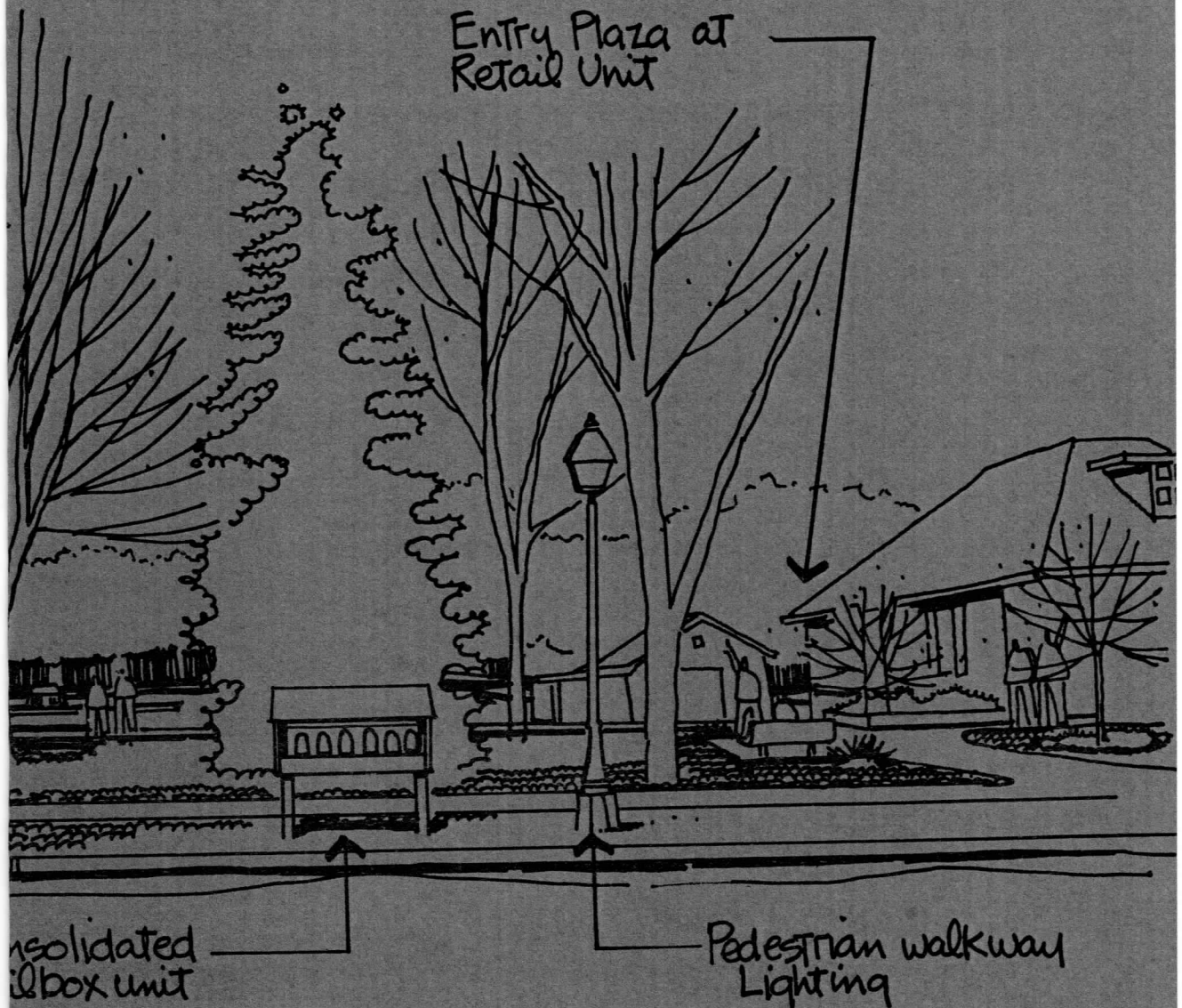
At the present time commercial expansion in Goodison is not a pressing

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Area To The rear
Rail Units

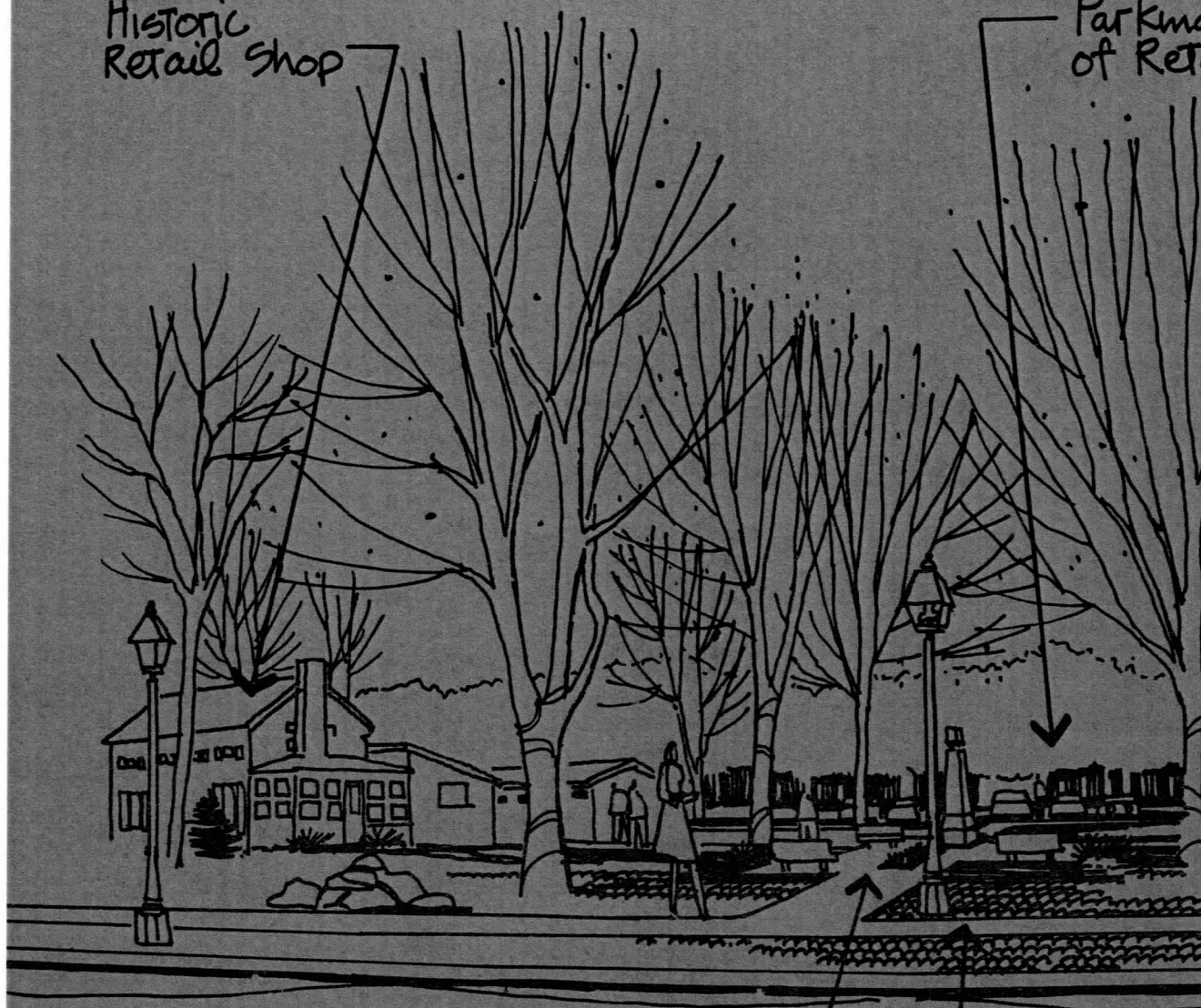
Entry Plaza at
Retail Unit



Historic / Commercial District

Historic
Retail Shop

Parking
of Ret.



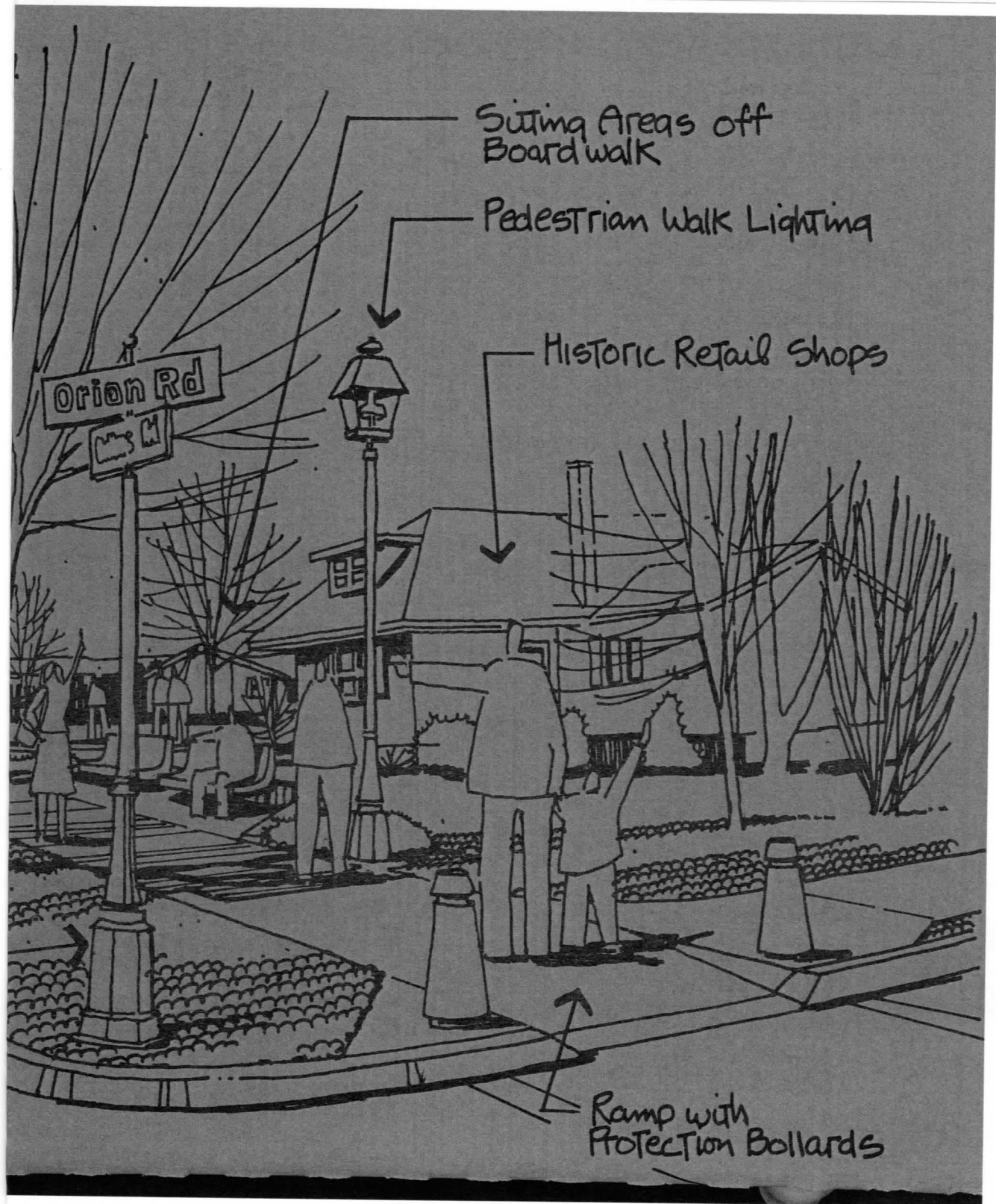
Connecting Walkway with Plaza.

Boardwalk

Co
Ma

The next two pages are halves of an 8-1/2" x 14" page of
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New Street Tree Plantings to
reinforce and supplement
existing plant materials

Consolidated mailbox
units



Wood Boardwalk

Street Sign and Lighting Standards
to reflect historic era design

Boardwalk

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

COMMERCIAL EXPANSION

Design Recommendations (continued)

Orion Road is classified office (O-1), totalling 11.3 acres. All of this area on either side of Collins, including 3 acres zoned commercial, is presently occupied by 14 single residential units with half of this number considered historical structures. The Goodison Plan recommends the elimination of the office classification and the designation of commercial (B-2) to both sides of Collins between Orion Road, Beatrice Avenue, and the Township Hall (Civic Center site). All but one of the residential structures within this area is of historical significance and has potential under the proposed commercial zone for a unique development of boutiques, shops, etc., fully utilizing all existing buildings. Parking should be located to the rear of shops, preserving the antique flavor of Collins Road frontage. Additional interest could be created here with the proposed boardwalk and the introduction of period street light posts. Successful examples of this type of development can be seen in Franklin, Petoskey, and Harbor Springs, Michigan.

SCHOOLS

Existing Conditions

The original Baldwin School located at Orion Road and Gunn Road was constructed in 1827 consisting of one room and a large fireplace. This structure functioned as a place of learning for 100 years until 1927 when a two room structure was built on the present site. To date three additions have been added providing community assembly facilities together with those required for elementary level education. The existing site covers 11 acres with two ballfields, a soccer field and a 2 acre nature interpretive area developed by Township residents. As noted previously, the site has no defined parking areas for either school or community functions and direct access from Collins Road from the east is awkward. School grounds also present a barren appearance with minimal landscaping and no hard surfaced walks.

Design Recommendations

As a major component of the Goodison Plan and as a part of the

11A

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

SCHOOLS

Design Recommendations (continued)

residential sector, the Baldwin School site should be given a large measure of consideration for certain site improvements. Principally recommended improvements are as follows:

1. Design and construct parking areas for both school and community use. This action should include turfed overflow areas.
2. Design and construct on site, hard surface walks as a part of the overall Goodison Walkway Plan. Costs for this project could be included with block grant funds now under application by the Township.
3. Improve present playgrounds and athletic fields for increased community use during off school periods.
4. Develop a master school landscape plan with emphasis on turf areas and shade tree plantings. A workable maintenance program should also be developed concurrently with the landscape plan. This plan should also cover improvements to the nature interpretive area including a detailed resource map and facility recommendations.

RESIDENTIAL USE

Existing Conditions

There are presently 25 building structures within the Goodison Plan Study

area in use as single family residences with the majority inhabited by their owners and fronting on Collins Road. The residential community of Goodison has remained stable throughout much of Goodison's history and current trends point to the continuance of this use. This conclusion is reinforced by the low density residential communities that are developing immediately around Goodison and throughout the Paint Creek Valley.

Unlike the larger homes being constructed in the surrounding area, residences within the Goodison area range from small to moderate averaging at approximately 1,200 square feet. Many are of historical significance and for the most part are well maintained by their owners.

Under existing classifications 13 residences are zoned office (O-1), 5 are zoned commercial and 1 is located within the industrial (L-1) zone immediately northwest of Rochester Gear on Orion Road.

Design Recommendations

The continuation of the residences in stable neighborhoods is a necessary component for any viable community and Goodison is no exception. In the interests of promoting this fact, the Goodison Plan recommends certain

RESIDENTIAL USE

Design Recommendations (continued)

zoning changes to protect the established residential land use while concurrently limiting commercial and industrial expansion to a scale compatible with the neighborhood concept desired by Goodison and Township citizens.

The principal area recommended for residential oriented zoning changes covers 5.5 acres between Collins Road and Springdale Avenue. Goodison Plan recommendations call for rezoning this sector from office (O-1) and commercial (B-2) to residential with Beatrice Avenue on the south as the dividing line between residential and commercial uses. The proposed "Village Green" at Beatrice and Springdale would function as a transitional use between residential and the adjacent industrial zone. This approach would provide the established residential area of Goodison with an improved relationship to other uses planned to the south and west (including Baldwin School) while matching up with developing residential to the north and east.

HISTORICAL PRESERVATIONS

Existing Conditions

As noted in the previous "History of the Goodison Settlement", Goodison along with Oakland Township possesses a cultural heritage worthy of addressing in any future planning efforts. Realizing this fact, citizens of both Goodison and the Township established the "Oakland Township Historic District Commission" consisting of five members appointed for 3 year terms each by the Oakland Township Board. The Commission acts in an advisory capacity to the Board and Planning Commission, however, it has the authority by ordinance to oversee and regulate historic structure construction, alterations, repairs, removal and demolition. The Commission also records and maintains a continuing survey of Oakland Township (including Goodison) historic districts, homesites, and landmarks.

The Historic Commission recently, through considerable time and effort, conducted a survey of historical homes within the Collins Road/Orion Road area including the Paint Creek Methodist Church. Survey results

ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

HISTORICAL PRESERVATIONS

Existing Conditions (continued)

clearly showed that residents favor the "historic" approach to any planning for Goodison and are anxious to retain the quiet, scenic, and small neighborhood scale of the area. In cases where historic residences presently occupy sites zoned for office or commercial, owners showed concerns about potential sale values but were not opposed to alterations providing historical character assuming costs were affordable.

Design Recommendations

The Township Historic District Commission should continue it's effort to not only preserve existing historical features but actively through the Planning Commission encourage new development to retain Goodison's historic environment wherever possible. The Historic Commission should be made aware of pending development and/or construction within all of Goodison's zoned areas so that they may have an opportunity to review all aspects of proposed construction regardless of the presence or absence of any historical designation.

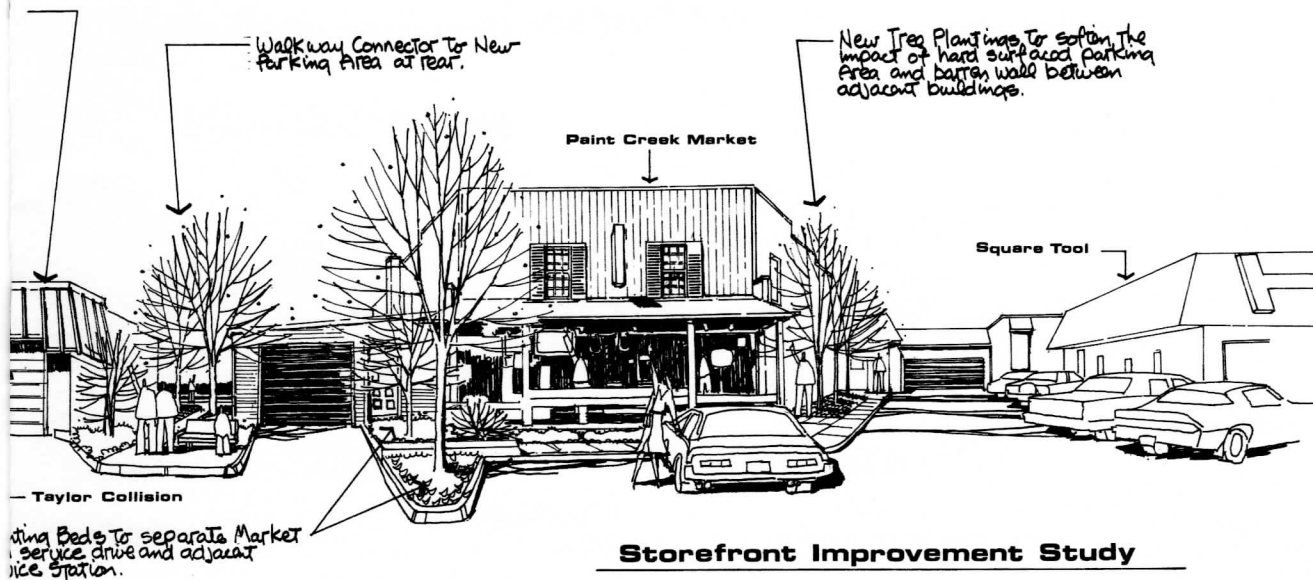
The Historic Commission is presently planning the restoration of two and possibly three of the Township's early structures consisting of two residences and a former D.U.R. Trolley waiting shelter. Relocation of these structures to the Township Civic Center site is also under consideration to ultimately create an "Antique Village". One of the residence structures known as the "Goodison House" would be restored to it's original condition to function as a museum, the other residence, the "Palmer House" would following restoration, provide headquarter space for the Historic Commission. This project upon completion as presently planned would certainly enhance Goodison's historic dimensions.

Other areas within the Goodison study area that deserve close attention toward advancing an early American motif are the following:

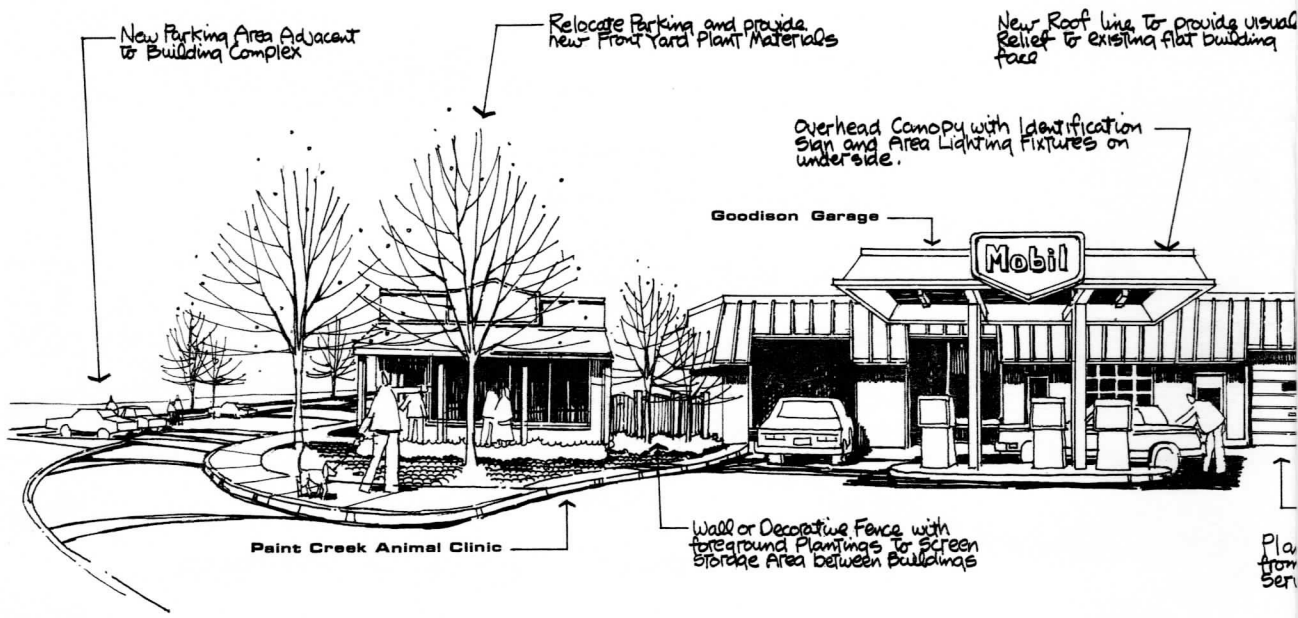
1. Pending conversion of the Paint Creek Cider Mill to a restaurant.
2. Alterations to existing commercial buildings,
3. Development of Township property along Paint Creek and the Mill Race.
4. Design of the "Village Green" within the proposed Goodison Plan.
5. Design and location of street lighting.

The next two pages are halves of an 8-1/2" x 14" page of
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Storefront Improvement Study



ISSUES AND DESIGN CONCEPTS PHASE TWO (continued)

HISTORICAL PRESERVATIONS

Design Recommendations (continued)

6. Design and placement of signs, both private and public.
7. Installation of historical markers.
8. Area landscape design.

In order to develop guidelines or criteria regarding the above and other items, the Historical Commission should develop a file covering examples of urban historic renovation in other communities and catalogues of manufactured products such as street lights, architectural materials, signs, etc. that are specifically designed for historic theme development. Such data would enable the Commission to formulate recommendations and alternatives for the Planning Commission's site plan review process. The Goodison Plan would act as a basis for such recommendations.

ENVIRONMENTAL RESOURCES

Existing Conditions

The scenic and natural resources of Oakland Township and the Paint Creek Valley that attracted Goodison's settlers during the early 1800's remain

in force today. Present residents are generally opposed to so called growth, preferring to retain the quiet village atmosphere characteristic of many country American villages. The urge to remain small and preserve precious natural resources is strong among Oakland Township residents and the pressure of developers desiring to capitalize on the area's potential is constant. Although such development in many instances is inevitable the Township has through the enactment of conservation of natural resources and wetland ordinances attempted to protect the area's natural environment.

Goodison's position in the Paint Creek Valley is a prime resource in itself, however, minimal attention has been given to the preservation of open space within the study area largely due to the abundance of such space in the surrounding undeveloped Township land. Proceeding on the realistic assumption that undeveloped land in its present form will not prevail, provisions need to be implemented to set aside open space if Goodison and the Township are to achieve the type of environment desired by the citizens. In addition to its resource oriented ordinances, the Township established a

ENVIRONMENTAL RESOURCES

Existing Conditions (continued)

Park Commission in 1974 and through this body has acquired 107 acres of hilly wooded land contiguous with the east boundary of the 11 acre Civic Center site on Collins Road. The Park Commission is also developing a master plan which will act as a guide for additional parkland acquisition throughout the Township.

Design Recommendations

The Goodison Plan recognizes the need for public open space at the immediate community level that would primarily be user oriented and simultaneously provide the desired balance among private land use zones. Filling the area with architecture and paving does not necessarily represent "progress" as traditionally interpreted in many established communities within metropolitan districts. Planned open space for Goodison focuses it's attention on the creation of a 2.2 acre "Village Green" neighborhood walk in park to be centrally located west of Collins Road at the intersection of Springdale and Beatrice Avenues. This area would function as a community commons for

passive activities such as walking, sitting or small group activities. Proposed facilities would be minimal including turf, shade trees, walks, benches, shelter and a historical exhibit. The Village Green would also supplement the adjacent Baldwin School site as a community activity area.

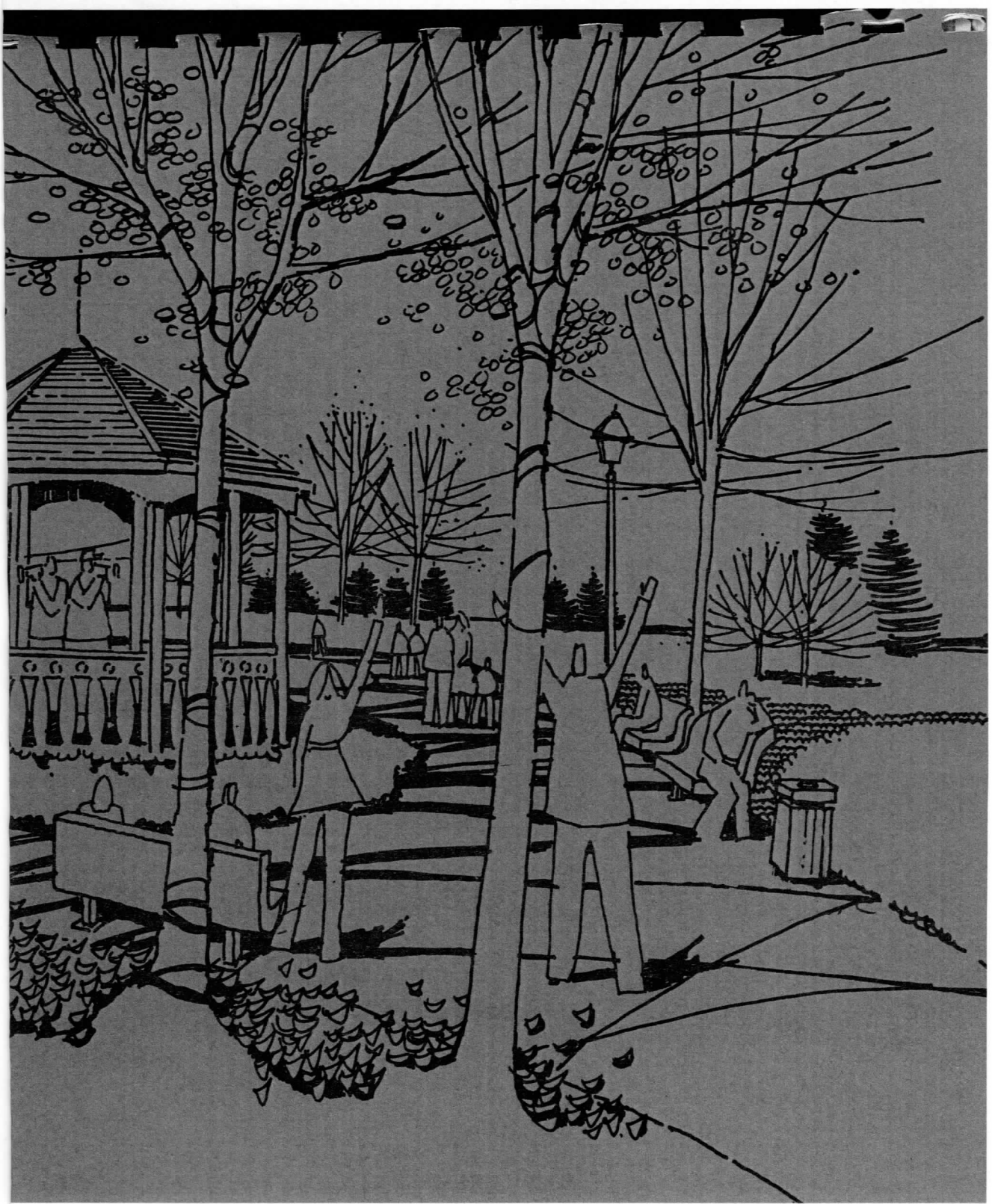
Another major component of Goodison's proposed environmental system is approximately 2.3 acres of Township land situated between Orion Road and Paint Creek immediately northwest of the Cider Mill. This property is heavily covered with brush and surrounds a segment of the mill race originally utilized by Needham's Mill for 100 years. Although deed restrictions prohibit severe alterations to the parcel's present natural character, it could be integrated into the walkway plan and be developed as a historically significant open space.

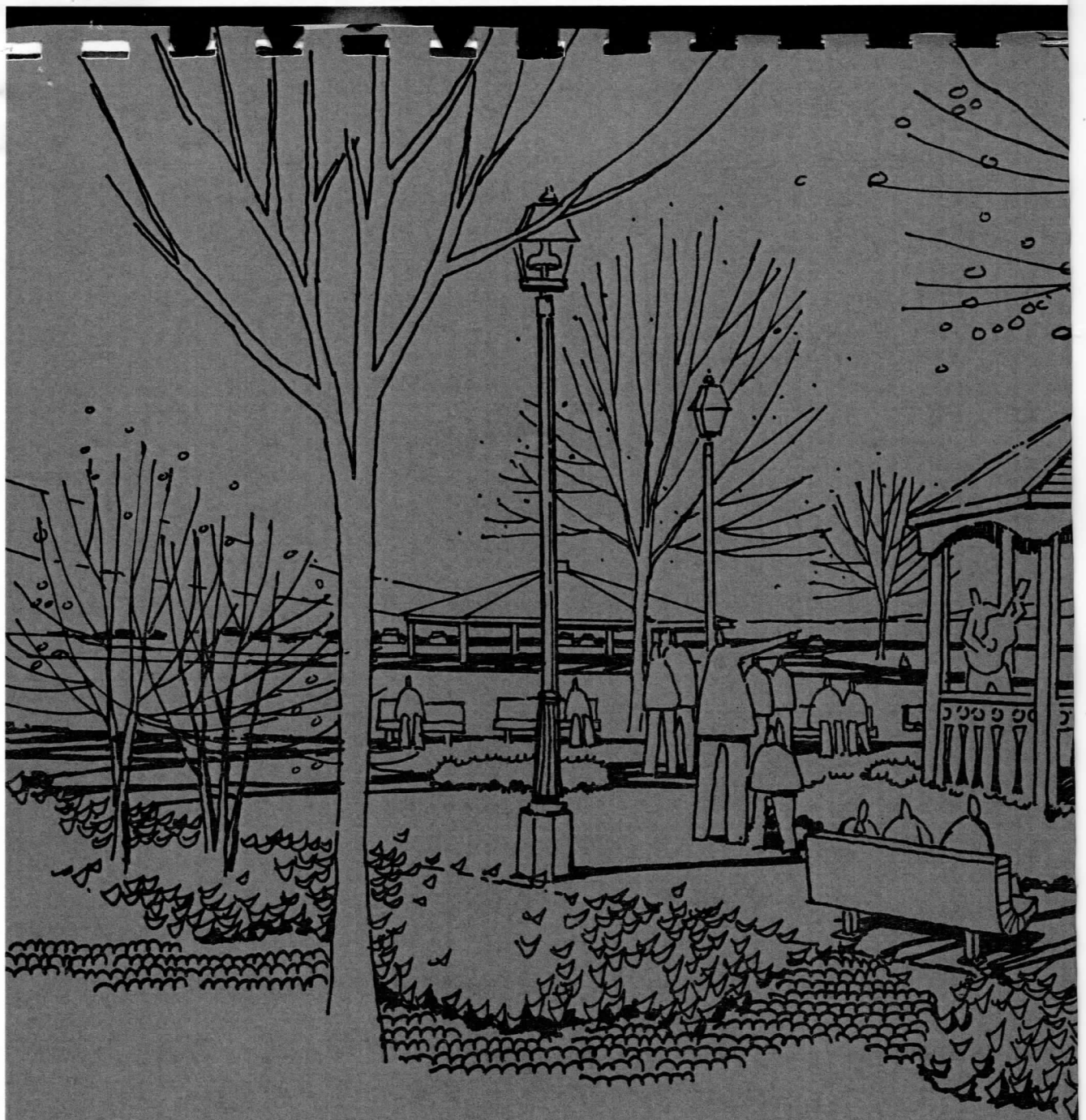
Additional potential environmental openspace within the Goodison area is situated along the margins of Paint Creek as shown on the plan utilizing a segment of the abandoned Penn Central Railroad right-of-way when it becomes available. Other environmental elements proposed on the plan include street and parking area shade tree planting, turf islands along walkways and landscape requirements for both new private sector construction and alterations to existing structures.

16A

The next two pages are halves of an 8-1/2" x 14" page of
Print and assemble as below:

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Village Green

PLAN PRIORITY RECOMMENDATIONS AND WORK PROGRAM

The following improvements as proposed within the framework of the Goodison Plan are listed below under a short term category (1 to 5 years) and a long range program (5 to 25 years).

SHORT TERM PLAN (1 TO 5 YEARS)

1. Complete necessary rezoning as per Goodison Plan recommendations. Following required public hearings and adoption incorporate the plan components as a part of the overall Oakland Township comprehensive master plan.
2. Continue to review all new developments within the Goodison study area against adopted plan concepts. Recent proposed development in this category include Lyon Gear expansion and the Paint Creek Restaurant.
3. Resolution of Bear Creek flooding as part of civic center development.
4. Implement construction of Beatrice Road and adjacent walkway as per detail shown in the Goodison Plan.
5. Following opening of Beatrice Road expedite closure of Territorial Road at Collins.
6. Construct segments of walkway plan as grant funds allow. High priority is recommended for Collins Road Boardwalks in connection with pending road improvements. Walkways from Beatrice to Baldwin School and the Paint Creek market are also recommended for prime consideration.

-
7. Design a series of signs utilizing a Goodison "theme" and logo identification.
 8. As a part of item #6, initiate walk and landscape improvements at the small lot adjacent to the Edison substation compound at the Springdale and Territorial intersection.
 9. Complete improvements to the curbed island at the Orion and Territorial Road intersection in front of the Goodison Garage.
 10. Implement first phase improvements within Paint Creek Park site. This work would include selective clearing, grading, and construction of walkway as per plan.
 11. Initiate acquisition procedures for securing proposed "Village Green" site. This project is expected to be funded in part through a federal matching grant under the Land and Water Conservation Program.
 12. Provide assistance to the Oakland Township Historical Commission toward the relocation of two and possibly three historical buildings on the Township Civic Center site. The general relocation area is shown on plan page.
 13. Existing commercial and/or industrial signs at variance with the Township sign ordinance should be corrected and all new development should be required to conform in the interest of the total Goodison environment.

Note: The above order of listing does not necessarily represent the recommended order of importance. It is reasonable to assume that some project may take precedent over others as available funding allows. In some instances certain projects may be concurrently implemented.

17A

PLAN PRIORITY RECOMMENDATIONS AND WORK PROGRAM

LONG RANGE PLAN (5 TO 25 YEARS)

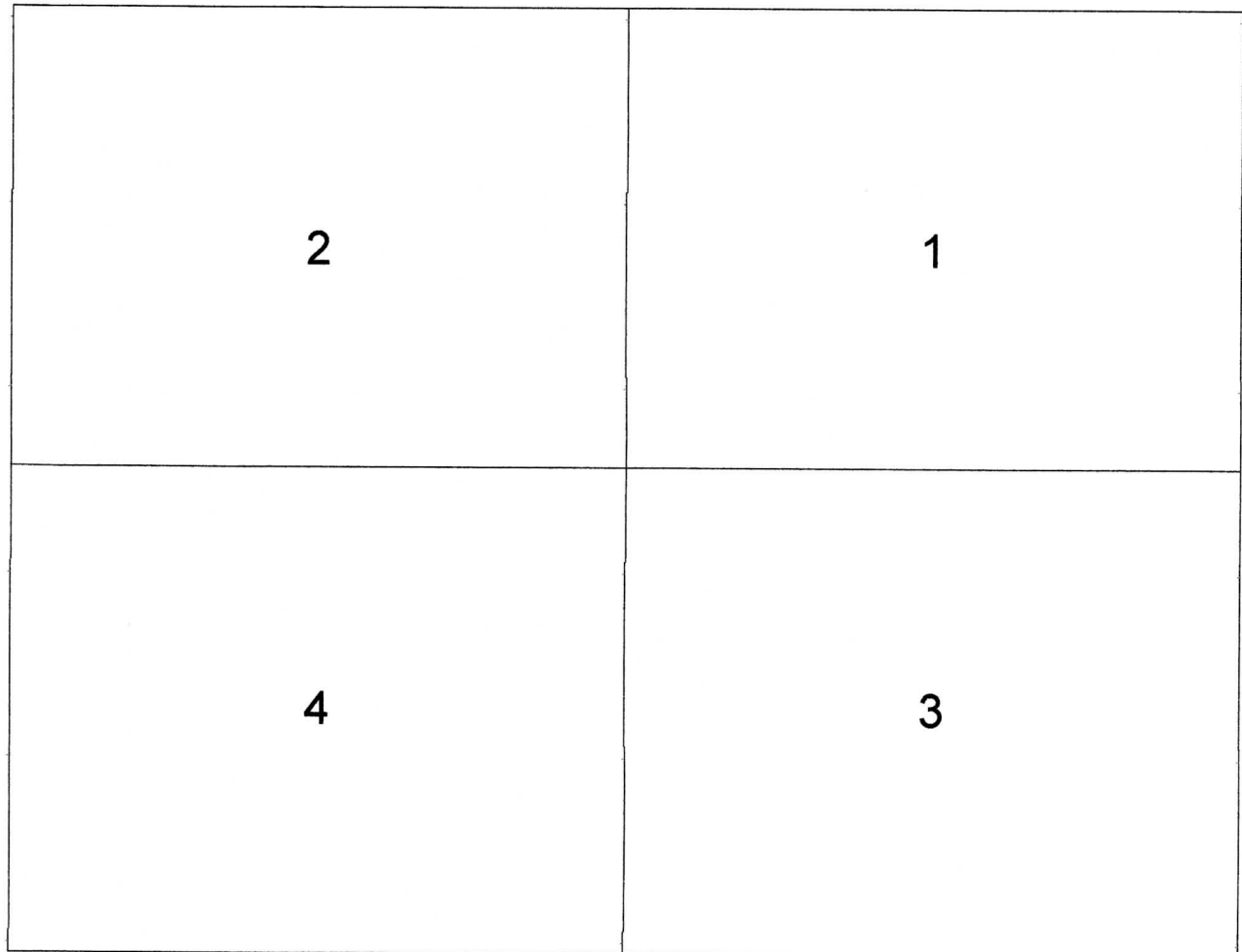
1. Complete acquisition of 2.2 acre Village Green site as per plan recommendations and construct facilities (walks, landscaping, etc.)
2. Complete all segments of the Goodison Area Walkway Plan including Paint Creek timber footbridges, Orion Road crossings, tie in to the Penn Central R.O.W., bikeway, and Village Green walk intersection plazas.
3. Complete development of 2.3 acre Paint Creek Park site. This includes walkways, terraces, shelter, historical marker and landscaping.
4. Continue efforts either through Township acquisition or private development toward the design and construction of vehicle parking lots within locations shown on the Goodison Plan. Lots should be designed for convenient access, have attractive lighting and landscaping.
5. Continue programs dealing with Goodison area environmental programs. Examples are street/walkway, shade tree plantings, industrial storage yard screening, promotion of placing overhead utilities underground insofar as possible, imaginative street lighting, elimination of free standing signs within commercial/industrial districts and development of public open space (Paint Creek Park, Village Green, etc.)

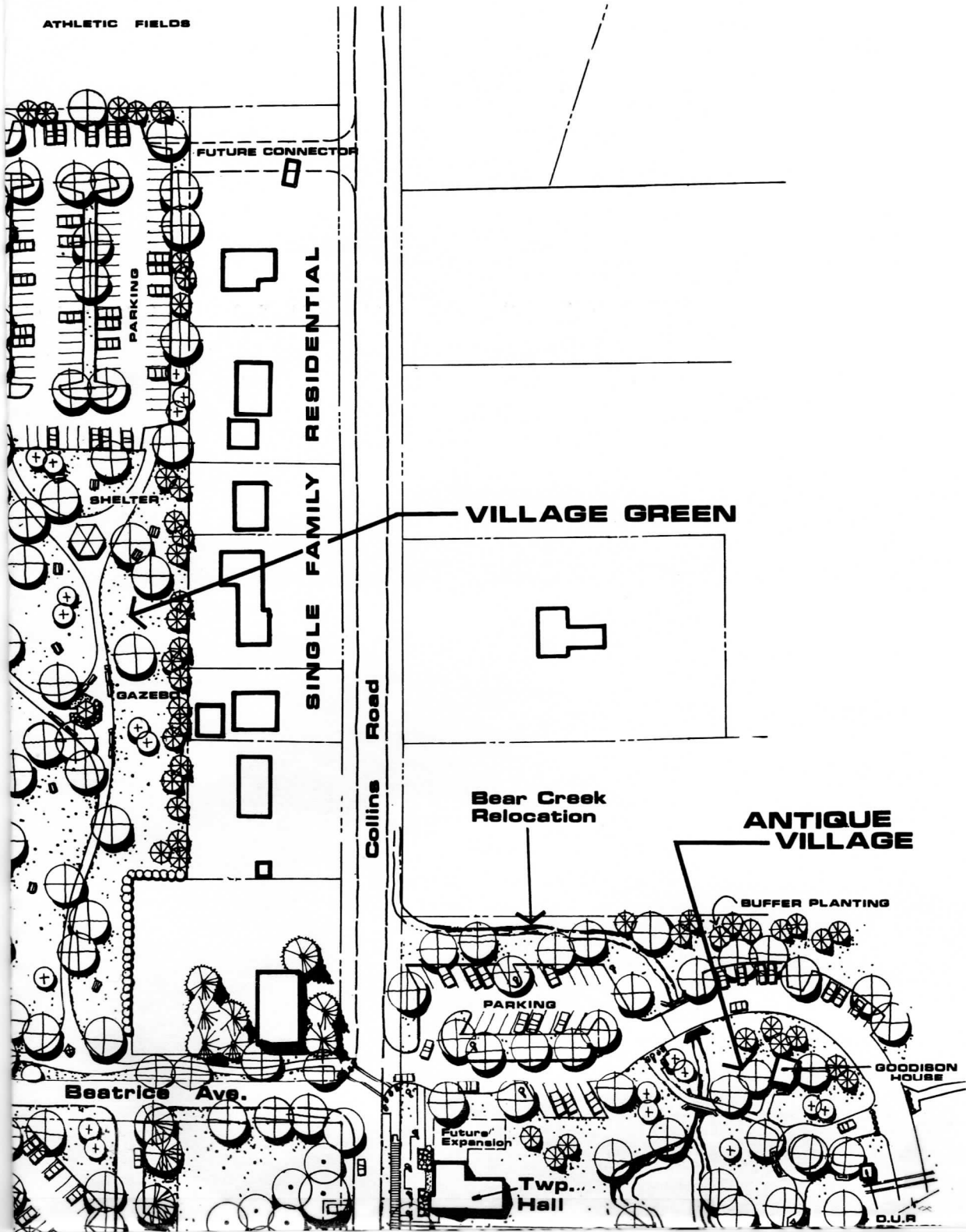
6. Maintain preservation through the Oakland Township Historical Commission of Goodison's historical structures, particularly those in designated commercial zones. As these historical buildings experience conversion from residential to various business uses, owners should be encouraged to preserve the buildings historical appearance in keeping with the objectives of the Goodison Plan.

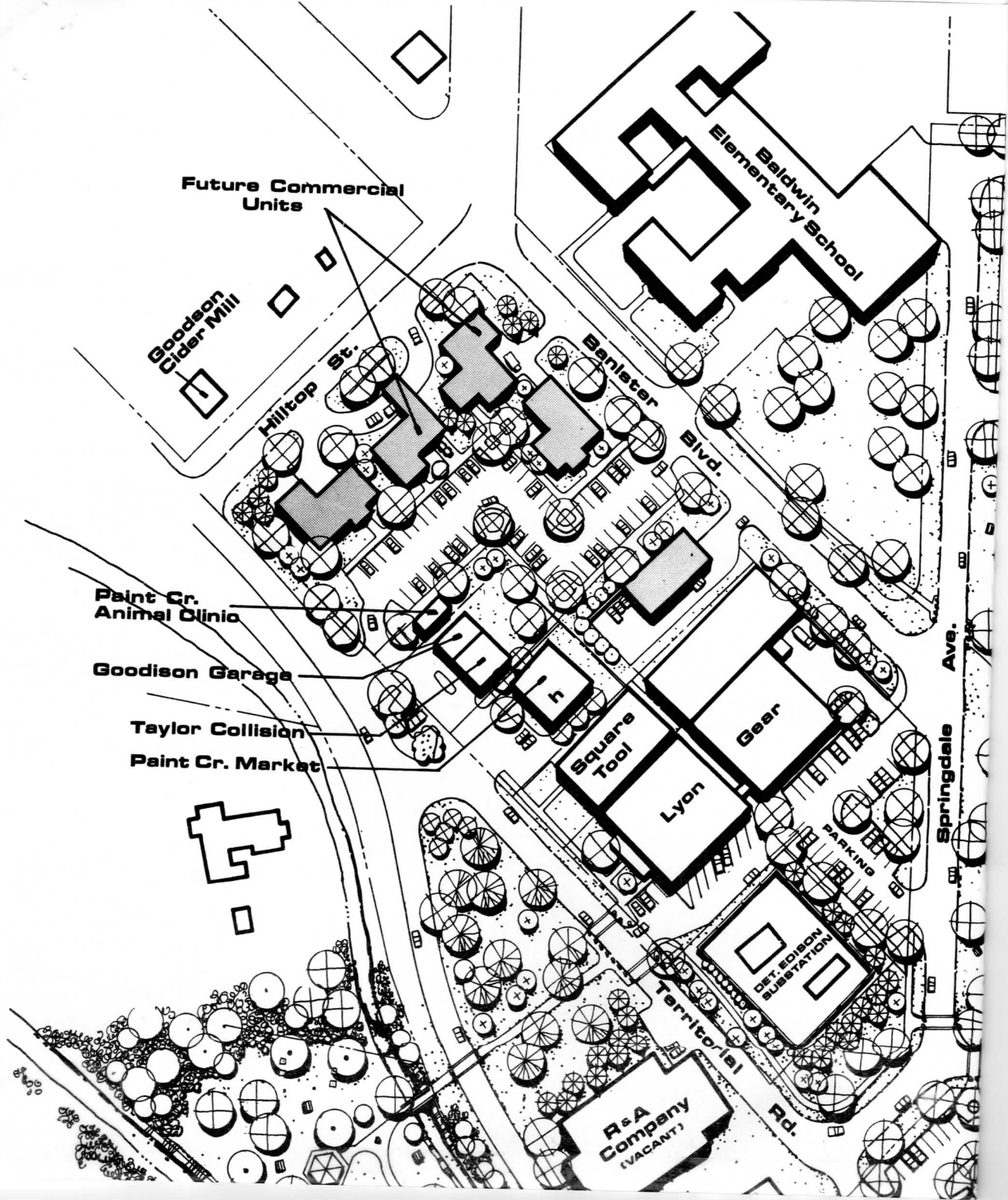


18A

The next four pages are four quadrants of a large blueprint size drawing. Print and assemble as below:







Future Commercial Units

Goodison Cider Mill

Hilltop St.

Baldwin Elementary School

Banister Blvd.

Paint Cr. Animal Clinic

Goodison Garage

Taylor Collision

Paint Cr. Market

Square Tool

Gear

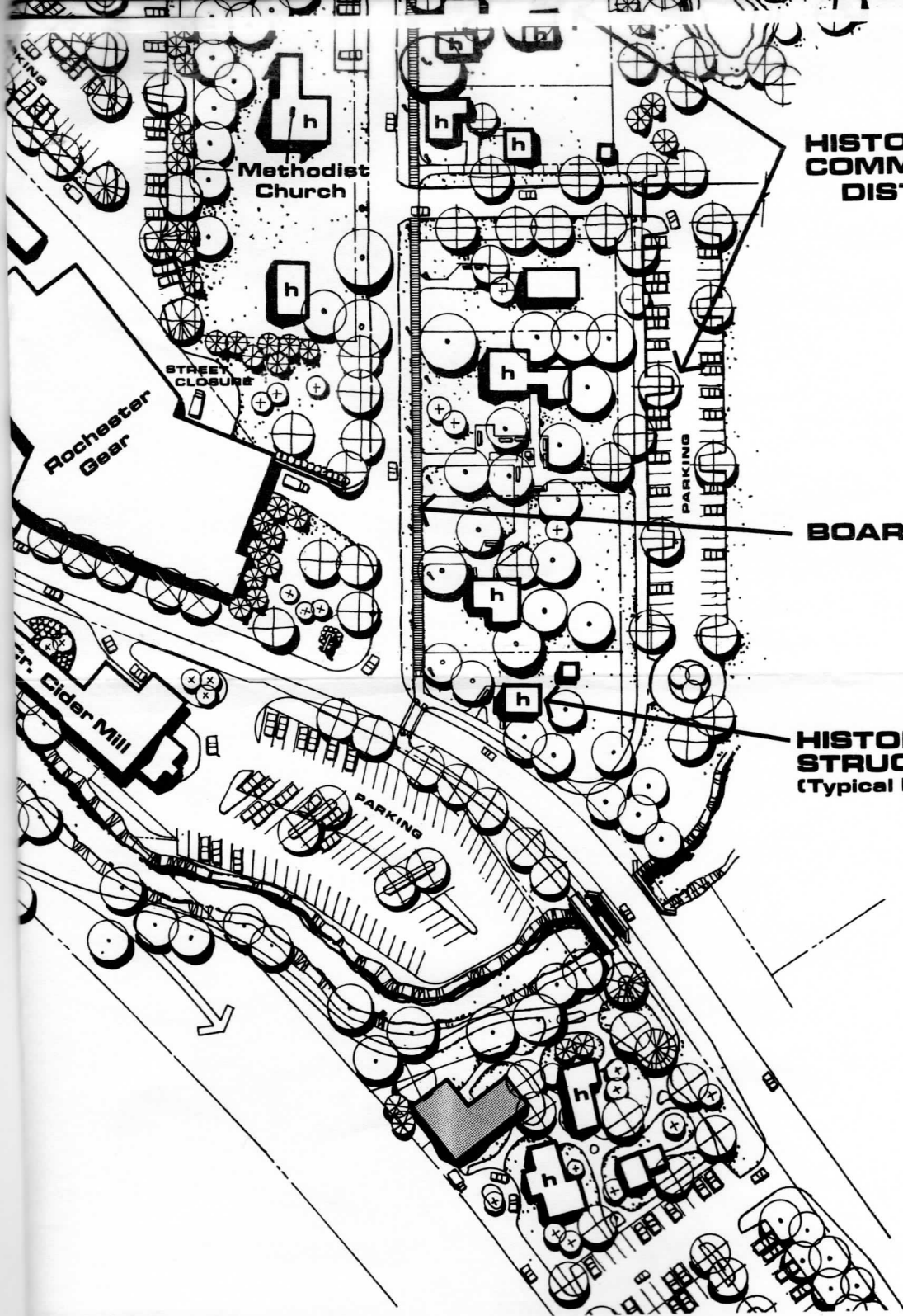
Lyon

DET. EDISON SUBSTATION

R&A Company
(VACANT)

Territorial Rd.

Springdale Ave.



**HISTORICAL/
COMMERCIAL
DISTRICT**

BOARDWALK

**HISTORICAL
STRUCTURES
(Typical Designation)**

**Methodist
Church**

**Rochester
Gear**

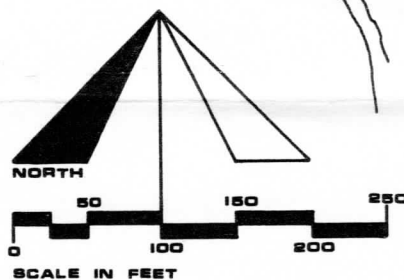
Cider Mill

PARKING

PARKING

**PAINT CREEK
PARK**

**PEDESTRIAN / BICYCLE PATH
(TYPICAL)**



PHASE II GOODISON PLAN

**TOWNSHIP OF OAKLAND
OAKLAND COUNTY, MICHIGAN**



BILLS/CHILDS ASSOCIATES, P.C.

5221 COMMERCE ROAD, ORCHARD LAKE, MICHIGAN 48033 313-582-7644

SITE DEVELOPMENT PLANNING • URBAN DESIGN • LANDSCAPE ARCHITECTURE
GOLF COURSE ARCHITECTURE • PARK PLANNING • ENVIRONMENTAL DESIGN

CONCLUSION

It is clearly evident that Oakland Township's present 7,000 residents including those within Goodison are seriously determined to preserve the desirable natural character and cultural heritage of their environs. Like the area's early settlers, today's residents find the scenic Paint Creek Valley and surrounding territory a pleasing location in which to live; often a welcome contrast to regional urbanized areas where they are employed and shop.

The purpose of the Goodison Plan is to assist in creating not only citizen awareness of their land use relationships but to provide a "how to do it" guide toward imposing workable limits on future growth. The task of remaining small is not easily accomplished; and a preconceived plan with majority support from all of those affected is absolutely necessary if uncontrolled growth is to be avoided. Franklin, Michigan, in south Oakland County has long been known as "the town time forgot" and yet has grown to a desirable neighborhood scale all in the shadow of intensively developed Southfield. This can only be achieved with organized citizen determination and continuous planning. Hopefully the

Goodison Plan will function as a starting point toward the kind of community identity it's present and future citizens may speak of as a prideful combination of old and new.



**CONTINUOUS SUPPORT IS NECESSARY
FOR SUCCESS**

19A